



## STATEMENT OF ENVIRONMENTAL EFFECTS

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Demolition of existing structures, excavation and construction of a commercial development involving the provision of an anchor supermarket, commercial and retail tenancies, public piazza and through-site link, with at-grade parking, associated landscaping and public domain works.

260 Eighth Avenue,  
Austral

Prepared for: Fabcot Pty Ltd

REF. M220565

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# 1. Introduction

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## 1.1 OVERVIEW

This Statement of Environmental Effects ('Statement') has been prepared on behalf of *Fabcot Pty Ltd*, the Applicant for the proposed development. The report is to accompany a development application to Liverpool City Council ('Council') seeking consent for the demolition of existing structures, excavation and construction of a commercial development involving the provision of an anchor supermarket, commercial and retail tenancies, public piazza and through-site link, with at-grade parking, associated landscaping and public domain works at No. 260 Eighth Avenue, Austral ('site').

More specifically, the proposal will remove any existing vegetation, excavate, and subsequently construct a commercial development comprising of commercial tenancies, retail tenancies, 2 kiosks and one anchor supermarket, with ancillary liquor tenancy and direct-to-boot service. In accordance with the intent of the Indicative Layout Plan (ILP), the design incorporates the provision of a public piazza orientated to Eighth Avenue, and north-south through-site link. The development also includes on-site at-grade parking for 303 spaces, including seven (7) accessible spaces, four (4) electric charging spaces and 4 direct to boot spaces. The proposal will also provide 4 motorcycle and 24 bicycle parking spaces and a shared loading bay.

Within the site boundaries, this application will also seek to undertake public domain works to Eighth Avenue, Auger Street and the western road, known as Warrawal Avenue. This will include the construction of roadways, pedestrian pathways, services, landscaping and on-street parking for 16 vehicles. In addition to this, public domain works are also proposed to the south-west of the subject site and will form part of a Works-in-Kind Agreement. These public domain works are located on the Council owned land, known as Lot 664 in DP 1260834, and connection point between Little Street to Warrawal Avenue. The proposal will seek to construct a 'new half road width' for the western half of the Warrawal Avenue, for the length of the lot 664 DP 1260834 and connection point between Little Street to Warrawal Avenue. This will be as a contribution, which is consistent with the *Liverpool Contributions Plan 2014 – Austral and Leppington North*.

This Statement demonstrates that the development meets the objectives of zone B1 Neighbourhood Centre and satisfies the relevant controls in the *State Environmental Planning Policy (Precincts – Western Parkland City) 2021* (Western Parkland City SEPP). Specifically, *Appendix 4 Liverpool Growth Centres Precinct Plan* applies to the subject site and has been considered in **Annexure A** of this Statement.

Further, this Statement evidences that the proposal generally aligns with the objectives and controls in the *Liverpool Growth Centres Precinct Development Control Plan* (LGC DCP), including *Schedule 1 Austral and Leppington North Precincts*, specifically, where the *Eighth Avenue Neighbour Centre* applies. As relevant, the *Liverpool Development Control Plan 2008* (LDCP 2008) has also been considered. Where there are variations to controls, these are justified throughout this Statement and the development remains consistent with the objectives of the controls.

## 1.2 SUPPORTING DOCUMENTS

This application is accompanied by the following supporting information:

- Architectural Plan Package – prepared by *Clarke Hopkins Clarke*;
- Design Report – prepared by *Clarke Hopkins Clarke*;



- Survey – prepared by *LTS*;
- Landscape Plan – prepared by *Ground Ink*;
- Bushfire Report – prepared by *ABPP*;
- Stormwater Documentation – prepared by *Henry & Hymas*;
- Civil and Road Works – prepared by *Henry & Hymas*;
- BCA Report – prepared by *Hontas, Hatzi & Co*;
- Access Report – prepared by *Hontas, Hatzi & Co*;
- Crime Risk Assessment Report – prepared by *Planning Ingenuity*;
- Social Impact Assessment – prepared by *Planning Ingenuity*;
- Electrical Investigation Report – prepared by *Power Solutions*;
- Essential Services Report – prepared by *Henry & Hymas, Power Solutions* and *Simpson Kotzman Engineers*;
- GreenStar Rating – prepared by *Northrop*;
- Acoustic Report – prepared by *Acoustic Logic*;
- Arborist Report – prepared by *Naturally Trees*;
- Waste Management Plan – prepared by *Elephants Foot*;
- Traffic Report – prepared by *Colston Budd Rogers & Kafes*;
- Geotechnical and Salinity Report – prepared by *Geo-Logix*;
- Detailed Site Investigation Report – prepared by *Geo-Logix*;
- ESD Report – prepared by *Northrop*;
- Aboriginal Heritage Assessment – prepared by *Austral Archaeology*;
- Odour Assessment – prepared by *Todoroski Air Sciences*;
- Quantity Surveyors Report – prepared by *TCubed Consulting*; and
- Quantity Surveyors Report (CIV Works) – prepared by *TCubed Consulting*.

### 1.3 PURPOSE OF STATEMENT

The purpose of this Statement is to address the planning issues associated with the development proposal and specifically to assess the likely impact of the development on the environment in accordance with the requirements of Section 4.15 of the *Environmental Planning & Assessment Act, 1979* (EP&A Act).

This Statement has been divided into six sections. The remaining sections include an analysis of the site and locality; a description of relevant background information; a description of the proposal; an environmental planning assessment pursuant to Section 4.15 of the EP&A Act; and a conclusion.



## 2. Site Analysis and Context

### 2.1 THE SITE

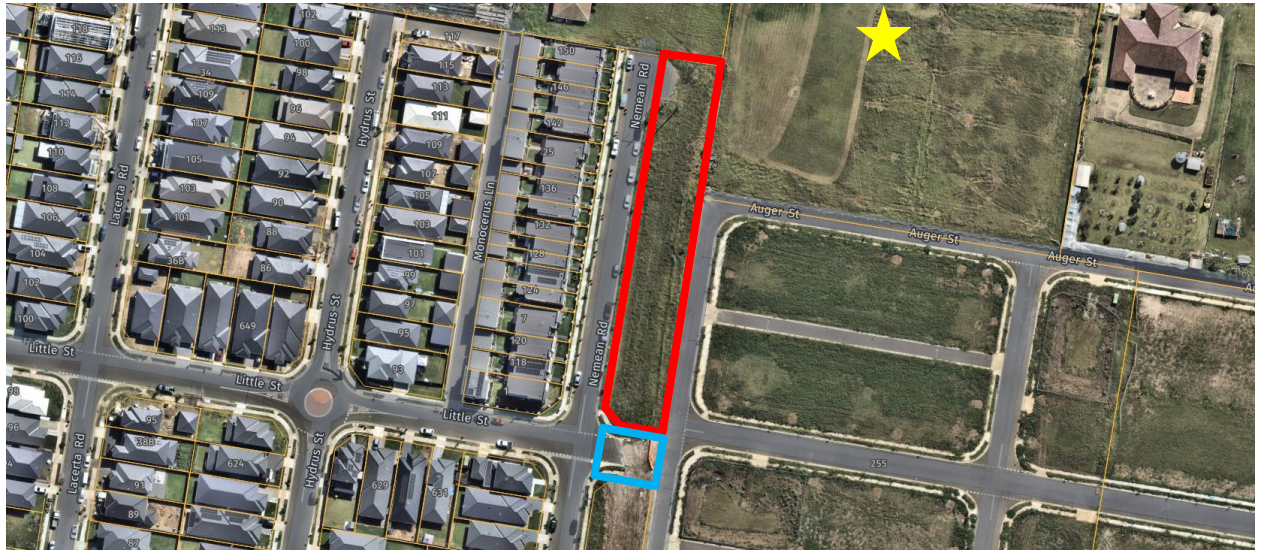
The subject site is known as No. 260 Eighth Avenue, Austral (also identified as Nos. 330-350 Eighth Avenue) and has a legal description of Lot 940 in DP 1265677. The site has a total area of 18,170m<sup>2</sup> (1.817ha). The location of the site is shown edged in red, per the aerial image provided at **Figure 1**.



**Figure 1** Aerial image of the subject site and surrounds

As detailed, this application will seek to undertake public domain works on behalf of Council in accordance with a Works-in-Kind Agreement. This will involve additional roadworks within the Council owned allotment known as Lot 664 in DP 1260834, specifically, the western half of the western road (Warrawal Avenue), for the length of the Council owned allotment. The proposal will also seek to provide works for the connection point between Little Street and Warrawal Avenue. This is shown in **Figure 2** below and is described in Section 4.10 of this Statement.





**Figure 2** Council owned allotment Lot 664 in DP 1260834 edged in red, connection point between Little Street and Warrawal Avenue in blue and site shown as star

The zoning of the subject site is demonstrated in **Figure 3** below. The subject site is zoned B1 Neighbourhood Centre, with R3 Medium Density Residential zoned land to the south and west, and RE1 Public Recreation and SP2 Infrastructure Educational Establishment to the north. Where the public domain works are proposed as they form part of the Works-in-Kind Agreement (WIK Agreement), the land is zoned SP2 Infrastructure Local Roads.



**Figure 3** Aerial image of the subject site and zoning, site edged in yellow, WIK works in blue

The site is rectangular in shape with a northern frontage of 120.545m to Eighth Avenue, a southern frontage of 120.28m to Auger Street and a western frontage of 150.875m to the proposed Warrawal Avenue. The site has an eastern boundary, shared with Lot 941 in DP2475 which is zoned B1, of 150.86m.



The site falls from the southern to northern boundary by approximately 4.5m across the length of the site and currently contains a dual frontage to both Eighth Avenue and Auger Street. As part of the Austral Precinct's Indicative Layout Plan (ILP), an additional roadway will be provided along the western boundary which will provide a third streetscape frontage. Furthermore, the ILP indicates that the subject site should accommodate a public piazza and pedestrian through-site link along the eastern (side) boundary, which connects Eighth Avenue and Auger Street. The site currently contains residential dwellings with detached metal sheds and sparse vegetation.

The site as viewed from Eighth Avenue is shown in **Figures 4 and 5** below.



**Figure 4** Subject site as viewed from Eighth Avenue in a south-easterly direction



**Figure 5** Subject site as viewed from Eighth Avenue in a south-westerly direction

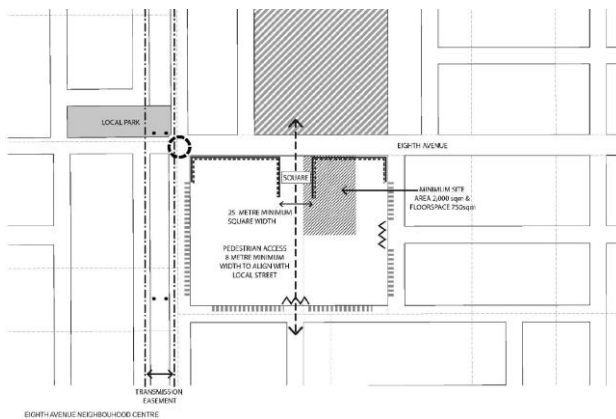
## 2.2 EXISTING AND DESIRED FUTURE CHARACTER

The subject site is located within the B1 Neighbourhood Centre zone which is anticipated to accommodate a significant increase in density in accordance with the Western Parklands City SEPP. The site is located within the Austral Precinct, specifically Eighth Avenue Neighbourhood Centre, which is expected to undergo a significant transformation as a result of the adopted planning controls, including the zoning, building height and building envelope.



The Eighth Avenue Neighbourhood Centre envisages the provision of a built form with active street frontages to Eighth Avenue, a public piazza and pedestrian through-site link (**Figure 6**). In accordance with the ILP, the overall allotment (as described in Section 2.1 above) includes the provision of an additional roadways, including Eighth Avenue, Auger Street and Warrawal Avenue, which will form part of this application (**Figure 7**). Further works are proposed to the roadways within the Council owned properties to the south-west of the subject site, as part of a Works-In-Kind Agreement.

As discussed in this Statement, the proposal is considered to achieve the desired objectives per the Western Parkland City SEPP (Chapter 3, Appendix 4) and Growth Centre DCP, to provide a high quality, contemporary built form.



**Figure 6** Eighth Avenue Neighbourhood Centre – Desired Future Layout (Extract from DCP) **Figure 7** Indicative Layout Plan

The locality generally consists of mixture of ageing, low-density residential dwellings on large allotments, open farmland spaces and rural structures. However, and within proximity of the site, are recently subdivided and constructed residential developments, including single dwellings and dual occupancies. As is evident by these developments, the existing character of the locality is undergoing rapid change and is beginning to reflect the desired future character, as is anticipated by the relevant controls. The preparation and implementation of the Western Parkland City SEPP anticipates an increased density of development along Eighth Avenue, with the subject site identified as a key nodal centre within the locality. Given the site's strategic location, the proposed development (as discussed in this Statement) reflects the desired future bulk, scale and character of development as is anticipated.

## 2.3 ACCESSIBILITY

The proposed development and subsequent increase in commercial uses is a direct result of the site's strategic location and B1 Neighbourhood Centre zoning. The site is situated within an accessible location and is opposite SP2 Educational Establishment land, RE1 Public Recreation land and is amongst R3 Medium Density Residential and R2 Low Density Residential zones. The desired increase in density on-site and within the neighbouring properties directly reflects strategic location of the site. The objectives of the Eighth Avenue Neighbourhood Centre seek to encourage a high quality precinct which provides access to shopping, employment and services for local residents, in which the proposal will actively satisfy.





## 2.4 SURROUNDING DEVELOPMENT

Directly to the north of the allotment on the opposite side of Eighth Avenue is SP2 Infrastructure Educational land, RE1 Public Recreation land and R3 Medium Density Residential land. Per the ILP, this includes a school, public open space, residential dwellings and a local roadway running in a north-south direction as accessed from Eighth Avenue. These parcels of land are generally vacant, with dwellings and ancillary structures located on the southern end of Nos. 245 and 345 Eighth Avenue (**Figures 8 and 9**). At Nos. 355-357 Eighth Avenue, the site has been approved for the subdivision of 1 allotment into 33 lots with new roads. Works have commenced as a part of DA-932/2020 *Demolition of existing structures, Subdivision into 33 Torrens title subdivision with new roads and associated stormwater works* (**Figure 10**).



**Figure 8** No. 345 Eighth Avenue as viewed from public domain



**Figure 9** No. 245 Eighth Avenue as viewed from public domain, with recently subdivided and constructed dwellings in background



**Figure 10** No. 355 Eighth Avenue as viewed from public domain, with works being undertaken per DA-932/2020

To the east of the subject site is Nos. 310-320 Eighth Avenue (also known as No. 250 Eighth Avenue) which contains a single storey residential dwelling (**Figure 11**). This land is zoned B1 Neighbourhood Centre where it directly adjoins the subject site, with R3 Medium Density Residential zoned land further to the east. This property is also required to provide a community facility which is to be dedicated to Liverpool City Council in accordance with the *Land Reservation Acquisition Map* and *Clause 5.1 Reservation Acquisition Authority* of the Western Parkland City SEPP.



**Figure 11** Nos. 310-320 Eighth Avenue (also known as No. 250 Eighth Avenue) as viewed from public domain

To the south of the subject and on the opposite side of Auger Street is No. 255 Seventh Avenue, which contains an approval for subdivision under DA 2018/377, described as '*Torrens Title Subdivision of Thirty Five (35) Residential Lots with Three (3) Residue Lots for OSD and Two (2) Super Lots (to excise other future land uses for R3 zoned land), and Construction of Roads and Associated Civil Works, Demolition of One (1) Dwelling and Associated Outbuildings, Dam-Dewatering and Site Remediation*'. Following the subdivision, these properties have been approved for the construction residential dwellings and a childcare centre. Further to the south-west of the subject site are similarly subdivided and recently constructed residential dwellings, located to the west of Nemean Road. Land developed to the south and south-west is shown in **Figure 12** below.





**Figure 12** Development to the south of the subject site

To the west of the subject site and on the opposite side of the proposed roadway (under the ILP) is R3 Medium Density Residential Land, including No. 360 Eighth Avenue and a parcel of land subject to a recent subdivision. No. 360 Eighth Avenue contains a single storey residential dwelling with ancillary farm structures located to the western portion of the site. An existing overhead electricity line (easement) is also located to the west of the site, as shown in **Figure 13** below.



**Figure 13** Properties to the west of the subject site, as viewed from Eighth Avenue

As shown the above and within the aerial images provided in **Figures 1** and **2**, numerous roadways have been constructed throughout the locality. This includes a mixture of half and full roadways which will serve the future development. As identified in Section 4 of this Statement, the proposal will also seek to construct additional roadways which will serve the subject site and surrounding locality.



## 3. Background

### 3.1 PRE-DA MEETING

A Pre-DA meeting was held with *Liverpool City Council* on 8 February 2023 under PL-98/2022 to discuss issues pertaining to the subject development and its response to the Western Parkland City SEPP and Growth Centres DCP. Points of discussion during the meeting related to the building design and siting, proposed uses, the ILP and relevant services. The following table summarises addresses each of the points raised in the letter.

**Table 1** Pre-DA Response

Comment	Response
<b>Planning</b>	
<i>The applicant must consider amalgamation with the adjoining site to the east which is zoned B1, in order to provide a coherent neighbourhood centre in the locality and in accordance with the envisioned precinct plan. This would also assist in the fair distribution of the maximum gross leasable floor space of 10,000sqm.</i>	Amalgamation is not proposed. This is discussed in Section 5.2.10 of this Statement.
<i>The pedestrian access path in accordance with the indicative Layout Plan (ILP) is to be constructed over the site and in accordance with the DCP. Where a variation to the ILP is made, sufficient justification addressing the requirements of the DCP is to be made.</i>	The proposal provides a through-site link and public piazza as required by the ILP. Whilst this is not in the exact location as set out in the ILP, this is acceptable per Section 5.2.7 of this Statement.
<i>An activated frontage is required along Eighth Avenue. It is encouraged to further activate the north-western corner of the site fronting the intersection and to avoid the expanse of blank wall to the public domain.</i>	The proposal activates the majority of Eighth Avenue through the provision of various uses. The north-western corner is not activated due to the topography of the subject site. This is offset through the provision of additional active frontages throughout the site, with the façade buffered through vertical planting.
<i>There is potential that the application may be required to be reviewed by the Design Excellence Panel based on the impact of the amenity the proposal would have in the locality.</i>	Noted.
<i>A social impact assessment is required for the development especially in regards to the liquor store use.</i>	Social Impact Assessment is submitted with this application.
<i>Signage proposed for the development is to be demonstrated on the plans and on separate plans for assessment. The applicant is also required to address the requirements of State Environmental Planning Policy (Industry and Employment) 2021 - Chapter 3 – Advertising and Signage.</i>	Signage for the proposed Woolworths is submitted with this application. The location of future tenancy signage is shown on the plans. It is noted that the future signage will form part of separate applications.
<i>The proposed plans are to demonstrate if the retail stores are to be separated into multiple shops or if they are one single premise. This would influence on the proposed parking rates for the development.</i>	The retail and commercial tenancies can be separated into multiple tenancies in accordance with the Complying Development Codes. The proposal includes an excess of parking spaces which will meet the requirements of the DCP, specifically noting that a smaller retail tenancy elicits a lower rate than larger tenancies.

**Table 1 Pre-DA Response**

<i>The proposed design is to incorporate photovoltaic cells on the roof as well as implement water sensitive urban design measures.</i>	Photovoltaic cells are proposed on the roof. Water sensitive urban design measures are implemented per the Stormwater Plans and ESD Report.
<i>The proposed development is likely to be captured as a Traffic Generating Development under the SEPP for Transport NSW/ NSW Roads and Maritime.</i>	A Traffic Report is submitted with this application.
<i>Car parking and bicycle parking is to be provided in accordance with the DCP as well as cycleways along the new collector road and Eighth Avenue.</i>	Car and bicycle parking is provided within the at-grade area, and which will satisfy the DCP. Bicycle parking is also provided within the public piazza. Shared pedestrian and cycle ways are proposed along Eighth Avenue and Warrawal Avenue, per the civil documentation.
<i>Landscape screening and buffers are required along the perimeter of the carpark area. It is recommended that a buffer with a sufficient width (approximately 5m) is provided to help screen the carpark from the public domain and adjoining residential developments. The landscape buffer is to provide a high quality landscape design which also incorporates tree canopy cover.</i>	The proposal provides landscaped zones within the car park, including additional vegetation scattered throughout which will provide a visual buffer to the public domain and surrounding residential uses. Refer to Landscape Plan submitted with this application.
<i>The carpark is to incorporate landscape bays between every 8-10 parking spaces to reduce the amount of hardstand surface and create a buffer within the carpark area. A tree is to be planted within these landscape bays.</i>	The proposal provides canopy trees to a portion of the at-grade parking area which will reduce the amount of hardscape and provide natural shading.
<i>The pedestrian access path should aim to be designed as a flat path with no sudden changes in elevation (i.e. requirement for stairs and lifts).</i>	Due to the topography of the site, stair and lift access is provided from Eighth Avenue to the through-site link. Level access is not possible and the provision of stairs and lift access from Eighth Avenue is superior to providing multiple steps in the built form which will not meet the relevant accessibility standards. The lift and stairs from Eighth Avenue are integrated to the public piazza.
<i>Visual and acoustic screening is required for the loading dock along the western boundary.</i>	The proposal provides a roller shutter and vertical planting to the loading dock to ensure visual and aural privacy is maintained. Refer to Acoustic Report.
<i>The proposal is not considered to be consistent with the envisioned precinct plan in regard to location of buildings and size of the plaza, which should be a minimum 25mx25m plaza square which is inviting and active, with an appropriate landscape design.</i>	The proposal provides a public piazza which is 12m x 20m. When combined with the neighbouring property, a public piazza of 12m x 40m will be provided. This will therefore satisfy the intent of the ILP with a generously sized public piazza and appropriate landscaping. It is also considered that the proposed arrangement is superior to a deep and narrow public square, as shown in the ILP, noting that the DCP does not stipulate a minimum size requirement.
<i>The applicant is to consider providing more natural light into the tenancies and proposed pedestrian link. Currently, the top floor is located at the eastern end of the development above the tenancies, only allowing for natural light through the north and south openings. The applicant is to</i>	The proposal has been designed so that the north-south through-site link is partly open to the sky as to maximise the solar access and ventilation.

**Table 1 Pre-DA Response**

<i>consider relocating the top floor to the west/north-west end which could allow for an open/ transparent roof over the pedestrian walkway area which would provide natural light into this main area and further activation of the north-west corner.</i>	The proposed retail and commercial tenancies will be orientated towards the through-site link and Eighth Avenue.
<i>Part 3 of Schedule 1 of the Growth Centres DCP is to be considered for controls relating to the centres as well as Part 5 of the main body of the DCP.</i>	This is addressed in <b>Annexure B</b> of this Statement.
<i>Timing for the acquisition of the community centre land is unknown and would likely take place at a time when the owner intends to sell or Council requires the facility to be made.</i>	Noted. The proposal will not prejudice this allotment.
<i>Timing for the school in the area would be at the discretion of NSW Education.</i>	Noted, as above.
<i>A referral will be made to Sydney Water who will indicate timing of wastewater infrastructure servicing. It is recommended that the applicant consults with Sydney Water prior to lodgement of a DA to gain an indication if the site can be serviced.</i>	Noted. Refer to supporting documentation submitted with this application.
<b>Traffic and Transport</b>	
<i>A Traffic Impact Statement addressing the traffic generation and parking demands of the proposed development and the proposed means of accommodating the demand and mitigating any impacts on the surrounding road network is to be submitted. The report needs to cover how the proposed development is to access the external road network and any improvements required as a result of the development. The assessment also needs to consider traffic volumes expected 10 years post opening of the shopping centre and identify the infrastructure improvements required to mitigate the impact. Pedestrian access and circulation is to be addressed.</i>	<p>A Traffic Report prepared by <i>Colson Budd Rogers &amp; Kafes</i> is submitted with this application. The Traffic Report has considered the relationship of the proposed development to existing and future road networks, in addition to traffic generation. Per the Report, the development is considered acceptable in this regard.</p> <p>Pedestrian access is considered in this Statement and the Access Report prepared by <i>Hontas Hatzi &amp; Co.</i></p>
<i>The development is to negotiate with the adjoining landowners and construct the full width of Warrawal Avenue ("New West Road"). The proposed half road construction of the road may not be adequate to accommodate the largest vehicles expected to access the loading dock and general traffic to be generated by the surrounding development. Council is unlikely to support a one-way traffic flow arrangement on the road.</i>	The proposal will construct half of the western road, Warrawal Avenue. Vehicle access along the western road to the parking area and loading dock has been considered in the submitted Traffic Report and Civil Documentation.
<i>The applicant will also need to construct the full width of Eighth Avenue and the remaining half of the road to the south.</i>	The applicant will construct the half road of Auger Street and Eighth Avenue, resulting in the full roadway being constructed.
<i>The intersection of Eighth Avenue and Warrawal Avenue is shown on the submitted drawing as signalised intersection, on the basis of the DCP (which was in turn based on analysis made a long time ago). The traffic report needs to assess this and recommend the appropriate treatment at the intersection. The analysis needs to consider traffic volumes 10 years post opening of the shopping centre. It is in the interest of the development to identify what facility will serve their interest best. Depending on the traffic assessment results, the applicant may be required to construct the facility or contribute towards it.</i>	This is not proposed as part of this application.
<i>Driveway access and parking provision are to comply with the DCP and AS2890 parts 1, 2 and 6. The loading dock driveway (45m) needs to be</i>	The loading dock has been considered with regards to the Australian Standards in the Traffic Report. It is noted that the entry and exit

**Table 1 Pre-DA Response**

*reconsidered with the view to separating the entry and exit driveways. This can improve pedestrian access along the street verge.*

have been separated and contain roller shutters.

*The DCP requires shared pathway to be provided on both sides of a collector road.*

This is provided to Eighth Avenue and Warrawal Avenue per the Civil Documentation.

*It should be demonstrated that all expected vehicles will be accommodated, including deliveries and garbage servicing.*

The loading dock will accommodate heavy rigid vehicles as detailed in the Traffic Report.

### **Land Development Engineering**

#### **Key Engineering Issues**

*Proposal indicates half road construction of "New West Road". It is noted that the development proposes a loading dock fronting this half road. The applicant is to address how the largest vehicle will navigate the loading dock area and half road.*

Turning circles of 14.4m and 19m vehicles within the loading dock and the western road are shown within the Traffic Report.

*Turning paths need to be provided to demonstrate that the half road is adequate otherwise full width road construction will be required with adjoining owner's consent to support the proposal.*

The half road width will accommodate 14.4m vehicles per the Traffic Report.

*Proposed loading dock driveway is approximately 45m wide, the driveway is to consider pedestrian access within the verge area. The applicant is to consider the option of separated entry/exit driveways*

Separate entry and exit driveways are proposed, as shown on the architectural set.

*Use of OLD DCP sections for the subject site is considered appropriate from a development engineering perspective*

Noted, refer to Civil Documentation and Traffic Report.

#### **Stormwater**

*Stormwater drainage for the site must be in accordance with Council's Development Control Plan. A stormwater concept plan shall be submitted with the application.*

Refer to Civil Documentation prepared by Henry and Hymas.

*The stormwater concept plan is to be accompanied by a supporting stormwater management report and calculations. The supporting report is to be prepared by an appropriately qualified practicing Stormwater Engineer outlining the collection, use, reuse and disposal method for stormwater drainage. The report is to demonstrate that the stormwater drainage proposal will not adversely impact on the adjoining and downstream drainage system, and any downstream properties or public places*

Refer to documentation prepared by Henry and Hymas.

*The proposed development and stormwater drainage system is to be designed to ensure that existing stormwater runoff generated from upstream properties (up to the 1% AEP) and draining into the site is conveyed through the site without adverse impact on the development (including proposed OSD) and adjoining properties.*

As above, refer to Stormwater documentation.

*On-site stormwater detention (OSD) is required for the development. The following is also to be addressed:*

The proposal provides an on-site stormwater detention within the loading dock area. OSD Calculations are provided within the submitted Civil Documentation prepared by Henry and Hymas.

*a) Be in accordance with Council's OSD Policy and OSD Technical Specification.*

*b) Electronic copy of the DRAINS model is to be provided to Council. Where an alternative electronic model has been used, all input data and results is to be submitted with the DA application.*

*c) Must be within common property and accessible from the street.*

*d) The developed 1%, 20% and 50% AEP peak flows are to be maintained at pre-development flows through the incorporation of stormwater detention and management devices.*

**Table 1 Pre-DA Response**

<i>A pre and post-development catchment plan is to be submitted. The on-site detention system must be within common property and accessible from the street without going through dwellings or private courtyards.</i>	Refer to submitted Civil Documentation which includes pre and post catchment plans.
<i>A water quality treatment device shall be provided in accordance with Council's Development Control Plan. A MUSIC model shall be submitted with the development application.</i>	MUSIC Modelling is included within the Civil Documentation.
<b>Traffic and Access</b>	
<i>The application is to be supported by a Traffic Report prepared by a suitably qualified person.</i>	As detailed, a Traffic Report is submitted with this application.
<i>The application must demonstrate that access, car parking and manoeuvring details comply with AS2890 Parts 1, 2 &amp; 6 and Council's Development Control Plan.</i>	The Traffic Report demonstrates that the proposed access, parking and manoeuvring complies with the relevant Australian Standards.
<i>The application shall be supported by turning paths in accordance with AS2890 clearly demonstrating satisfactory manoeuvring on-site and forward entry and exit to and from the public road.</i>	Traffic Report includes turning circle diagrams in accordance with the relevant Australian Standards.
<i>Complete Stage 1 – Concept design Road Safety Audit and provide report as part of DA submission.</i>	This can be included as a condition of consent.
<b>Roadworks and Road Reserve Works</b>	
<i>The development will require the following external road works:</i> <ul style="list-style-type: none"> <li><i>o Half road construction along Eighth Avenue frontage.</i></li> <li><i>o Half road construction along South Road Frontage.</i></li> <li><i>o Half width road construction along "New West Road" is required however further.</i></li> </ul>	The proposal will: <ul style="list-style-type: none"> <li>- Construct the remaining half of Eighth Avenue;</li> <li>- Construct the remaining half of Auger Street;</li> <li>- Construct half the western road.</li> </ul>
<b>Earthworks</b>	
<i>No retaining walls or filling is permitted for this development which will impede, divert or concentrate stormwater runoff passing through the site.</i>	Noted.
<i>Earthworks and retaining walls must comply with Council's Development Control Plan.</i>	This is addressed in <b>Annexure B</b> of this Statement.
<i>The application is to be supported by a geotechnical report prepared by a suitably qualified person to address potential salinity and acid sulphate soils.</i>	A Geotechnical Report prepared by <i>Geo-Logix</i> is submitted with this application. This includes discussions regarding salinity.
<i>Proposed fill material must comply with Council's Development Control Plan.</i>	Noted, this can be conditioned upon approval.
<b>Environmental Health</b>	
<b>Statement of Environmental Effects</b>	
<i>The Application shall be supported by a detailed Statement of Environmental Effects (SEE) in accordance with the Environmental Planning and Assessment Regulation 2021.</i>	-
<b>Waste Management</b>	
<i>The application shall specify how refuse and waste will be managed during demolition, construction and operation. Suitable waste storage facilities are to be provided as part of the proposal</i>	A Waste Management Plan prepared by <i>Elephants Foot</i> is submitted with this application.
<b>Construction Phase Sediment and Erosion Control Plan</b>	
<i>A sediment and erosion control plan shall be prepared and submitted to Council for the construction phase of the project.</i>	Erosion and Sediment Control Plan prepared by <i>Henry &amp; Hymas</i> .



**Table 1 Pre-DA Response**

**Stage 1 - Preliminary Site Investigation**

*The proposed development may be impacted by contaminated soils. The State Environmental Planning Policy (Resilience and Hazards) 2021 Clause 4.6 requires a consent authority when assessing a development application to consider whether the land is contaminated, it is suitable in its contaminated state for the purpose for which the development is proposed to be carried out, and/or if the land requires remediation, it is satisfied the land will be remediated before the land is used for that purpose.*

A Detailed Site Investigation Report prepared by Geo-Logix is submitted with this application. This Report finds that the site can be made suitable for the retail development, following remediation of the land per a Remedial Action Plan.

*A suitably qualified and experienced contaminated land consultant is to prepare a Stage 1 - preliminary investigation for the property.*

As above.

*If contaminating activities are suspected or known to have occurred, or if the site history is incomplete it may be necessary to prepare a Stage 2 – Detailed Site Investigation.*

As above.

*Where the Stage 2-Detailed Site Investigation indicates that the site poses unacceptable risks to human health or the environment, a Remedial Action Plan (RAP) shall be prepared*

A Remedial Action Plan is required, and this can be imposed as a condition of consent.

**Acoustic Assessment**

*The proposed development may be a source of offensive noise and potentially impact upon human health and amenity. An acoustic report shall be prepared by a suitably qualified acoustic consultant in accordance with the NSW Environment Protection Authority's 'Noise Policy for Industry' (2017). The cumulative effect of noise must be considered when assessing the impact upon receivers.*

An Acoustic Report prepared by *Acoustic Logic* is submitted with this application and it relates to the impacts created by the development on neighbouring properties.

**Construction Noise and Vibration Assessment**

*Should the development include an underground basement carpark, given the close proximity to existing residential dwellings, a site-specific Construction Noise and Vibration Assessment prepared by a suitably qualified acoustic consultant shall be submitted for review. The Construction Noise and Vibration Assessment must include an assessment of expected noise impacts and detail feasible work practices to be adopted to avoid, remedy or mitigate construction noise and vibration impacts.*

The proposal will not provide basement parking and therefore a Construction Noise and Vibration Assessment is not submitted.

**Odour Assessment**

*Council's records indicate that the proposed development may be impacted by odour as a result of surrounding land uses that generate odour. As a result, a suitably qualified and experienced air quality consultant is to undertake an assessment to identify the impacts odour may have on the development.*

An Odour Assessment prepared by *Todoroski Air Sciences* is submitted with this application.

**Food Premises Construction Details**

*Detailed floor and section plans for the food premises are to be submitted to Council for review.*

Not applicable. Fit out will be subject to separate application and the exact operation of each tenancy is unknown at this stage.

**Mechanical Ventilation System**

*Detailed plans and specifications of the mechanical ventilation system servicing the food preparation and dining areas of the premises shall be prepared by an appropriately qualified person.*

This is to be imposed as a condition of consent as mechanical ventilation will likely change during design development.

**Waste Storage**

*The garbage/waste storage area/s shall be clearly identified on the site plan and be located within the proposed building. The designated garbage/waste storage area shall comply with the following requirements:*

As detailed, a Waste Management Plan is submitted with this application. Conditions of consent can be implemented where required.

**Table 1 Pre-DA Response**

a) The room shall be fully enclosed and provided with a concrete floor, and with concrete or cement rendered walls coved to the floor;	The waste storage rooms will be finished with appropriate materials.
b) The room shall have a floor waste which is to consist of a removable basket within a fixed basket arrestor and is to comply with Sydney Water requirements; and	Refer to submitted documentation.
c) The door to the room must be tight-fitting, self-closing and fitted with mechanical ventilation.	As above.

#### **Floodplain Management**

The proposed development site is located within the Bonds Creek catchment. Existing flows running through the site shall be captured and managed into the stormwater system of the proposed subdivision, and the development shall not have any adverse flooding impact on the adjoining properties.	Refer to Civil Documentation prepared by Henry & Hymas.
The stormwater pipe system of the proposed subdivision shall have adequate capacity to convey upstream post-development flows.	As above.
During the 1% AEP storm event, the depth of flooding and velocity*depth on the road shall not be higher than 0.2m and 0.4 m <sup>2</sup> /s respectively.	As above.
Interim on-site detention (OSD) basin shall be provided to limit the post-development flow to the pre-development level. The interim OSD shall be maintained until the adjoining lots are developed, and stormwater networks are established. The OSD basin cannot be removed without Council's written consent.	An OSD bank is provided for the development and is shown on the Civil Documentation.
On-site water quality treatment facilities shall be incorporated in the proposal to ensure that stormwater runoffs leaving the site comply with Council's water quality standards.	As above.
Streetscape raingardens are required at the locations marked as "Proposed Raingarden Locations" presented in Schedule 1 - Austral & Leppington North Precinct DCP.	Refer to Civil Documentation.

#### **City Design and Public Domain**

The proposed extent of the loading dock frontage is not supported. Figure 3-5 of Schedule 1-Austral and Leppington North Precinct DCP (the DCP), states that 'Loading areas are not to be visible from the street' including along the frontages of New West Road and South Road. The loading dock should be positioned in a way that it is not visible from the street frontage. The loading dock also needs to minimise impacts to any pedestrian lines of travel through the installation of pedestrian priority crossings along the footpath (i.e., levelled footpath).	The proposal has been designed to provide separate entry and exit to the loading dock, with roller doors and vertical planting provided to soften the loading dock as viewed from the public domain.
CDPD recommends the applicant to explore orienting the loading dock on an east/west axis sleeved into the built form and minimise the extent of loading dock visible to the street. The loading dock could be consolidated with the car park entry/exit to minimise impacts to pedestrian's path of travel.	This is not proposed as it will significantly impact the feasibility of the development. The proposed arrangement is the most efficient in terms of operation and compliance with the relevant Australian Standards for access and manoeuvring.
The applicant is to ensure that the proposed loading dock also includes screening measures to minimise the visual impact on the street frontage. Noting that the DCP states 'loading areas that are adjacent to residential zoned land are to include visual and acoustic screening to protect the	The proposal provides roller doors and solid wall with vertical planting as to protect the amenity of future residents on the western side of Warrawal Avenue.

**Table 1 Pre-DA Response**

*amenity of residents.' Provide adequate screening and/or buffer measures for the loading dock area.*

*The development application should include details of the proposed paths of travel, access points, rest areas and activation zones within all planned public space, its connection to the street network and contribution to the existing public domain.*

Refer to the architectural set and relevant documentation.

*The applicant needs to provide more detail into the types of uses i.e., retail, commercial and community proposed in the speciality retail stores and amenities that will achieve the objectives of the DCP.*

This is to be subject to a future application. The exact uses of these spaces are unknown at the time of preparing this development application. Dedication as retail and commercial tenancies will enable fit out via Complying Development Certificate, under Part 5 and Part 5A of Codes SEPP.

*The proposed car parking should be screened from all sides through dense landscaping. The DCP requires that active street frontages are to be provided as per Figure 3-5. Entry/exit ways should be condensed to ensure minimal disruption to pedestrian lines of travel*

The proposal provides street tree planting and planting throughout the parking area so that it will be softened as viewed from the public domain.

*There is concern the proposed piazza/laneway does not provide a quality, attractive environment to the neighbourhood centre. As the primary entrance of the site, the piazza should provide clear paths of travel into and through the site for people of all levels of ability. This space has the potential to be an activated laneway that connects South Road with the school on Eighth Avenue and as such should be clearly defined and activated with retail frontages.*

The proposal provides a generously sized public piazza which will provide direct access via a lift and stairs to the through-site link and rear of the site. This is considered acceptable as the through-site link is generously sized and partly open to the sky which is considered acceptable. It is also noted that the site falls steeply from the south to the north, therefore requiring vertical circulation from the public piazza to the through-site link.

*The laneway should be designed in a manner that provides a safe and alternative walking route through the area, strengthening connections to the school, park centre and surrounding residences. Ensure that CPTED principles are considered as part of the design of the laneway that remains consistent with the objectives of the DCP.*

As detailed above, the through-site link is provided from Eighth Avenue to Auger Street, is generously sized and partly open to the sky so that it is attractive and easily accessible. The through-site link will be clearly visible from the public domain and will be activated through numerous uses.

*The speciality retail located along the eastern boundary of the proposal is not supported. There is concern the speciality retail along this boundary is not consistent with Figure 3-5 of the DCP and will impact the development on the adjoining site. This proposal should clearly identify the relationship with the adjoining Lot 941 zoned B2. Provide context within all components of the development application. The detail landscape drawings, architectural statement and visual assessment summary must include visual context of the adjacent lot to ensure a cohesive design.*

The proposed specialty store along the eastern boundary is considered acceptable as it will respond appropriately to any future development on the neighbouring property, as shown in the submitted architectural plans. This is also discussed in Section 5.2.7 of this Statement.

*The intersection of Eighth Avenue and New West Road should provide a direct entry point into the neighbourhood centre. This is to allow for direct access from the RE1 site located to the north-west and should support any commercial and retail uses in the local centre. The activation of this northwest corner needs to be addressed as part of any future proposal and for the economic viability of the town centre.*

The proposal does not provide any access from this corner of the site. This is considered to be reasonably offset through the generously sized public piazza and through-site link. It is also noted that the corner entry is not prescribed by the DCP Indicative Layout Plan and is inherently impacted by the site topography.

**Table 1 Pre-DA Response**

<i>As a local neighbourhood centre, any proposals along Eighth Avenue, New West Road and the proposed South Road will set a precedent for public domain treatments within Austral. The proposed development must consider its contribution of quality open space for the future residents of Austral. Refer to the Western Sydney Street Design Guidelines on guidance for public domain treatments.</i>	The proposal will include public domain improvements which will form part of the Civil Documentation submitted with this application.
<i>The proposal should incorporate buffer planting along the frontages of the proposed development (i.e., Eighth Avenue, New West Road and South Road), where stopping/parking is not permitted or where cycle paths are situated.</i>	The proposal will include street tree vegetation and vertical planting to the facades as to soften the built form.
<i>The DCP identifies both Eighth Avenue and New West Road as the location for proposed/future off-road cycleway routes. Include infrastructure to support off-road cycle routes within public domain proposals.</i>	Refer to Civil Documentation which includes shared pedestrian and cycle ways in accord with the DCP.
<i>As part of the development application, clearly identify a cyclist's path of travel to safely reach the bike storage. Consider providing separate access for cyclists to bypass gated vehicle entry points and avoid lobby areas.</i>	Bicycle parking is provided clearly within the parking area and public piazza, away from the vehicle parking. This will ensure safety of future cyclists.
<i>The overall design of the local centre should ensure the supermarket is situated within the core of the centre and smaller speciality retail stores located on the periphery to provide for an economically vibrant neighbourhood centre. Refer to Edmondson Square town centre for best design outcome.</i>	The supermarket is located within the core and smaller retail and commercial tenancies are located on the periphery, through site link and public piazza, which activates the surrounding locality.
<i>The proposed development should identify the location of any electrical substation (if required) and incorporate the required area within the landscape design. If a pad mounted substation is selected, it should be oriented in a way that the shorter face of the substation is aligned to the adjacent road and there are minimal impacts to visual amenity along the street.</i>	An electricity substation is provided per the submitted documentation.
<i>The development application should include cross sections and detailed landscape drawings for the project. The landscape drawings should be prepared by a suitably qualified AILA registered Landscape Architect in accordance with the latest industry standards and best practice guidelines.</i>	Landscape Plans prepared by <i>Ground Ink</i> are submitted with this application.
<i>The detailed landscape drawings should include all proposed works to the public domain with the context of surrounding sites including the consideration of the adjoining Lot 941. The applicant should refer to the Western Sydney Street Design Guidelines for best design outcomes when designing the public domain.</i>	As above, landscaped plans are submitted with this application.
<i>Landscape Plans should include all proposed species, soil depths, plant location/quantity, materials and finishes and indicate all maintenance access routes as part of the Development Application.</i>	As above.
<i>To assist council with DA assessment, clearly identify the location of each plant and trees species within the proposed design, as part of the landscape drawings. All street trees should be planted at a minimum 100L pot size and trees closer to the intersection should be planted at a minimum 200L pot size.</i>	As above.
<i>The development application should include a summary of all sustainability principles being incorporated as part of the project</i>	ESD Report prepared by <i>Northrop</i> is submitted with this application.

**Table 1 Pre-DA Response**

<i>Any future development application needs to ensure there is adequate canopy cover throughout the site. Austral is at high risk to the effects of urban heat. Council would expect that a significant site like this to achieve 40% tree canopy cover in line with the Premier's Priorities.</i>	The proposal provides a canopy cover of 15.7% within the site, after boundary adjustment.
<i>The development should integrate permeable spaces as part of the development to further assist in mitigating the effects of urban heat.</i>	Permeable surfaces are provided where possible.
<i>The applicant should provide an architectural design statement for the proposed façade treatments for the development including all materials and finishes.</i>	Refer to Materials and Finishes Schedule and Design Report submitted with this application.

#### **Strategic Planning**

<i>The Austral and Leppington north (ALN) Precinct Planning Report was prepared in 2011. The report informed the current DCP and include schematic diagrams for planned centres in Austral. The figure below is an early iteration of the vision for the Eighth Avenue neighbourhood centre. Key principles include vertical pedestrian plazas/squares (aligned with streets) and consolidation of adjoining lots.</i>	The proposed building arrangement and design is discussed in this Statement and supporting documents.
<i>The above plan was amended in 2013 and can be found in Schedule 1 (section 3.1.4) of the Liverpool Growth Centre Precinct DCP (see below). The key principles have been retained in the latest schematic diagram (i.e., vertical pedestrian plaza/square, community centre and consolidation of adjoining lots).</i>	Refer to Section 5.2.7 of this Statement.
<i>The proposed plans for Woolworths are inconsistent with the above diagrams. The plans do not include a vertical pedestrian plaza/square or consolidation of adjoining lots</i>	As above.
<i>The applicant must demonstrate that they have attempted to consolidate their site with the adjoining neighbour to the east to ensure there is an appropriate interface to any vertical pedestrian plaza/square</i>	Supporting diagrams are provided which demonstrates that any future development to the east will complement the proposed development.
<i>Plans should be amended to reflect future consolidation of sites (i.e., a vertical pedestrian plaza/square should be incorporated along the eastern boundary of the subject site).</i>	As above.

## **3.2 DESIGN DEVELOPMENT**

The proposed scheme has been developed following a detailed site analysis, examination of various built form solutions and has undergone a number of iterations in order to achieve the best possible outcome for the site. The iterations have taken into consideration the following:

- Reducing potential adverse impacts to the surrounding locality;
- Providing a high-quality built form to contribute to the urban environment and public domain;
- The suitable provision and layout of tenancies;
- Amenity for future occupants;
- Safety and access for pedestrians and vehicles; and
- Operational efficiency of the development.

Testing of the built form has been critical in accordance with the DCP per the Eighth Avenue Neighbourhood Centre. Given the topography, size of the site, multiple street frontages and relationship to the surrounding locality, the proposal has been designed in a manner to ensure compatibility with the desired character of the locality. The proposal will not





preclude future development on the property to the east and will ensure the amenity of existing and future developments will be appropriately retained. As discussed in this Statement, the proposal seeks a variation to the desired ILP envelope contained within the DCP.





## 4. Description of the Proposal

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### 4.1 OVERVIEW

This application proposes the demolition of existing structures, excavation and construction of a commercial development involving the provision of an anchor supermarket, commercial and retail tenancies, public piazza and through-site link, with at-grade parking, associated landscaping and public domain works. The proposed built form is one to three storeys in height and is designed to present to the public domain.

The proposal will include the provision of retail tenancies and two kiosks, commercial tenancies, an anchor supermarket with associated liquor store and Direct-to-Boot service. The fit out and use of the commercial and retail tenancies will form part of separate applications.

Integrated into this is a 'public piazza' orientated to Eighth Avenue, with north-south 'through-site link' which provides pedestrian access to Auger Street, via the at-grade car park. The at-grade parking area is orientated to Auger Street and Warrawal Avenue and will provide for 303 car parking spaces, including 7 accessible spaces, 4 electric charging spaces and 6 direct to boot spaces (DTB). A loading bay is also provided as accessed from the Warrawal Avenue.

The proposal will also seek to undertake public domain road works to Eighth Avenue, Auger Street and the future Warrawal Avenue. Additional road works to the Council owned land to the south-east of the subject site will be proposed via a Works-in-Kind Agreement, as discussed further in this Statement.

The proposed development is depicted on the plans prepared by *Clarke Hopkins Clarke*, which are included with the development application and described in detail below.

### 4.2 EXCAVATION AND DEMOLITION

The proposed development seeks to demolish all existing dwelling and structures on the subject site. All structures required to be demolished will comply with *AS 2601-1991: Demolition of structures*.

Earthworks are proposed in order to enable redevelopment of the site. The extent and degree of excavation has been reduced where practicable to maintain the topography of the site as it relates to the neighbouring properties and the public domain. Throughout the site, the excavation will enable the provision of the following:

- At-grade parking area;
- Building pads and ancillary structures;
- Vehicular access from future Warrawal Avenue and Auger Street; and
- Pedestrian access from the public domain and throughout the site.

A Geotechnical Report prepared by *Geo-Logix* accompanies the application and provides details of geological conditions on the site and recommendations for the excavation.

### 4.3 THROUGH-SITE LINK AND PUBLIC PIAZZA

As detailed under Section 4.1, the proposal provides a north-south pedestrian through-site link and public piazza. The following is noted:



- *North-south through-site link*; This provides access from Eighth Avenue to Auger Street. This element is activated through the public piazza, building entries and numerous tenancies; and
- *Public Piazza*; This directly fronts Eighth Avenue as required by the ILP and is shared with the property to the east. This element is activated by various tenancies, the building entry, landscaping and will be a key feature of the site.

## 4.4 ACCESS AND PARKING AND SERVICING

The proposal will seek to provide on-site, at-grade parking area for a total of 303 vehicles. This will include 7 accessible spaces, 4 electric vehicle charging spaces and 6 direct to boot spaces. The proposal will also provide for 24 bicycle spaces throughout the site and 4 motorcycle spaces in the parking area. In addition to this, 16 on-street parking spaces are proposed along Eighth Avenue and Warrawal Avenue, as part of the public domain works.

Vehicular ingress and egress to the on-site at-grade parking area is provided via two vehicular cross overs, one from the future Warrawal Avenue and the other from Auger Street. These entrances are provided separately to remove conflict, improve traffic flow and ensure the safety of future users and the general public. Primary pedestrian access to the subject site is provided from Eighth Avenue and Auger Street via the public piazza, through-site link and at-grade parking area. Addition pedestrian access is provided via a walkway from the future Warrawal Avenue.

The proposed loading dock has been designed to accommodate 19m heavy rigid vehicles, simultaneously. In the interim, the loading dock will operate as detailed in the submitted Traffic Report.

## 4.5 BUILDING ARRANGEMENT AND CONFIGURATION

The proposed development is described on the submitted architectural plans prepared by *Clarke Hopkins Clarke*. The northern portion of the site is occupied by built form, whilst the southern portion is occupied by the at-grade, on-site parking area.

### Lower Ground Floor:

The lower ground floor is designed to address Eighth Avenue and is located partially below natural ground as the topography rises towards Warrawal Avenue. Within the north-eastern corner of the site, the proposal will include the provision of the public piazza and north-south through-site link addressing the public domain, which will include a variety of landscaped elements and 14 bicycle parking spaces. The north-south through-site link will provide for stair access to the ground floor, with lift access provided via the lobby.

The proposal will also include the provision of commercial floor area, specialty retail floor area and a shared lobby, which provides vertical lift access to the upper levels. The anchor supermarket team area is also provided on this level, as accessed from the upper ground floor above. The remainder of the lower ground floor includes services and facilities necessary for the centre to operate.

### Upper Ground Floor:

The upper ground floor is located predominantly above ground level as it fronts Eighth Avenue, however, provides level access as it adjoins the future Warrawal Avenue and Auger Street. The upper ground floor includes the provision of the pedestrian north-south through-site link, which provides access from Eighth Avenue through to Auger Street, via the on-site parking area.

The upper ground floor will include the provision of an anchor supermarket, with ancillary liquor premises and direct-to-boot service. The anchor supermarket is accessed via the north-south through-site link and will be directly connected



to the loading area (accessed from Warrawal Avenue) via the goods receiving area. Vertical stair access is provided to the lower and upper levels to the supermarket team area and building services, respectively.

The remainder of the upper ground floor includes the provision specialty retail tenancies and two kiosks, orientated to Eighth Avenue, the parking area and through-site link. The proposal will also include the provision of a lobby area (with lift access to upper and lower levels), shared amenities and loading bay. The on-site parking area is described in further detail below.

#### **At-grade parking area**

The on-site at-grade parking area is directly accessible from the upper ground floor. This area will provide for 303 parking spaces, which will include 7 accessible spaces, 4 electric vehicle charging spaces and 6 direct to boot spaces. Vehicle access to the parking area is provided via two cross overs from Auger Street and the future Warrawal Avenue and will also include parking for 4 motorcycle and 10 bicycle spaces.

The parking area will also provide pedestrian access via the north-south through-site link and from the future Warrawal Avenue, and will include a variety of landscaping and facilities necessary for the development to operate. The proposed vehicular access and parking arrangement has been designed to comply with AS2890.1.

#### **Level 1:**

Level 1 will include the provision of commercial floor area, which is orientated towards Eight Avenue, the public piazza, through-site link and on-site parking area. The commercial tenancy will include shared amenities and will be accessible via the lobby, which provides lift and stair access to the lower levels. The remainder of level 1 includes the provision of services necessary for the building to operate.

## **4.6 COMMERCIAL AND RETAIL TENANCIES**

The fit out of the proposed specialty retail and commercial tenancies will form part of separate applications. These tenancies can also be subdivided under separate Complying Development Certificates in line with *Part 5 Industrial and Business Alterations Code, Subdivision 1 Building Alterations (Internal)* under *State Environmental Planning Policy (Exempt and Complying Development Codes) 2008*. Subdivision 1 permits internal alterations where it will not result in any additional gross floor area, which can be achieved.

## **4.7 TREES AND LANDSCAPING**

Pockets of deep soil landscaped areas are proposed throughout the site which will include a variety of vegetation. The proposed landscaped areas contain sufficient area and depth capable of supporting a variety of vegetation, including mature trees, to improve the character of the site, provide shading and improve permeability. The proposed landscaping will include mature trees located within the public piazza, through-site link and within the at-grade parking area. Soft landscaping and vertical planting are also provided to soften the built form and improve the character of the site.

In addition, the proposal will include the provision of street tree planting along Eighth Avenue, Warrawal Avenue and Auger Street. The landscape design concept is detailed in the Landscape Plans prepared by *Ground Ink* and submitted with the development application.

## **4.8 SIGNAGE**

The proposed development will seek approval for signage as it relates to the future Woolworths anchor supermarket, BWS liquor tenancy, direct-to-boot service and for wayfinding. This includes on-building identification signage, at-grade



and cantilevered pylon signage, parking signage and direct-to-boot signage. This is discussed in further detail under Section 5.2.4 of this Statement in relation to the SEPP (Industry and Employment) 2021.

## 4.9 ARTWORK

The proposal will seek to erect an artwork on the north-western corner of the built form. The artwork will be considered post approval and can form part of a condition of consent.

## 4.10 WATER MANAGEMENT

Stormwater Plans and Civil Documentation prepared by *Henry and Hymas* have been prepared and are submitted with the development application.

## 4.11 OPERATIONAL DETAILS

The following operational details are proposed:

- **Centre:** The proposed centre will operate from 6am to 12pm, all week;
- **Woolworths Supermarket:** The proposed supermarket will operate from 6am to 12pm, all week;
- **BWS:** The proposed BWS will operate from 6am to 12pm, all week;
- **Commercial tenancies:** The proposed commercial tenancies will operate from 6am to 12pm all week;
- **Retail tenancies:** The proposed retail tenancies will operate from 6am to 12pm, all week; and
- **Loading dock:** The proposed loading dock will operate from 7am to 12pm, all week.

The number of staff and fit out of the commercial tenancies and retail tenancies will form part of separate applications.

## 4.12 PUBLIC DOMAIN WORKS

The proposed development will seek to undertake public domain works as follows:

- **Eighth Avenue:** Upgrade of half the road width along the northern boundary. This roadway will meet the minimum collector road requirements as set out in the DCP and intersect with Warrawal Avenue;
- **Western Road (Warrawal Avenue):** Construction of half road width along the western boundary, which will connect to both Eighth Avenue and Auger Street. This roadway will meet the minimum collector road requirements as set out in the DCP; and
- **Auger Street:** Construction of half road width along the southern boundary. This roadway will meet the minimum local street requirements as set out in the DCP and meet Warrawal Avenue.

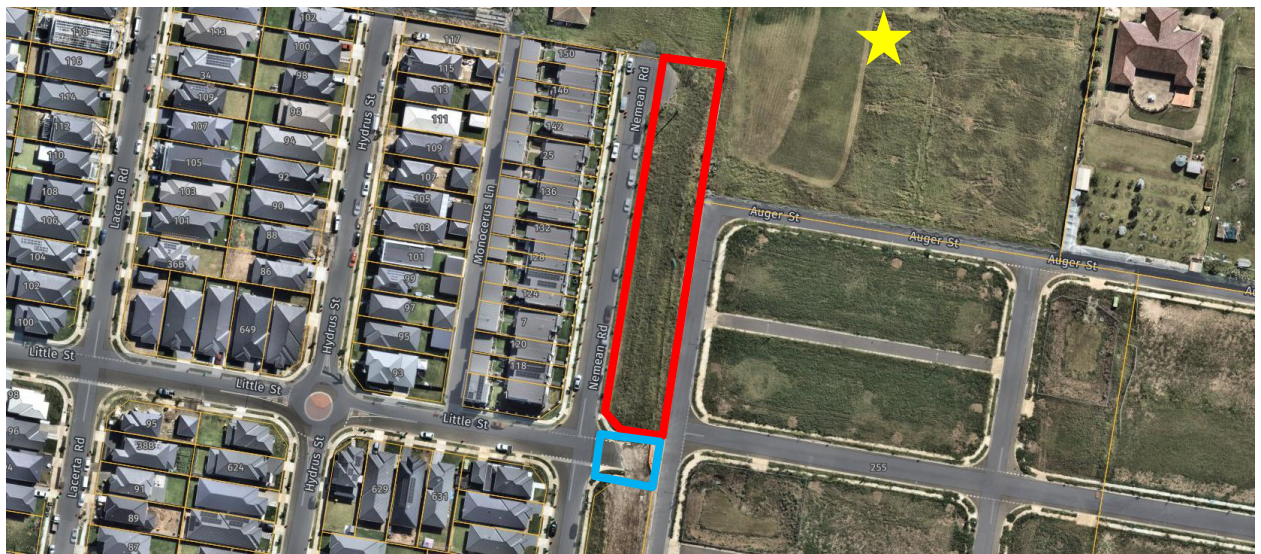
Per the above, all public domain works will include roadways, kerbing, pathways, drainage and associated landscaping and will be undertaken by the Fabcot Pty Ltd as required by the ILP. These works are shown on the Civil Documents prepared by *Henry & Hymas* and are to be constructed in accordance with the relevant Australian Standards.

In addition to the above, the proposal will seek to undertake additional public domain works within the Council owned allotment identified as of Lot 664 in DP 1260834, situated within the western half of the Warrawal Avenue (for the length of the allotment), and for the connection point between Little Street and Warrawal Avenue. This is discussed in further detailed under Section 4.12 of this Statement.



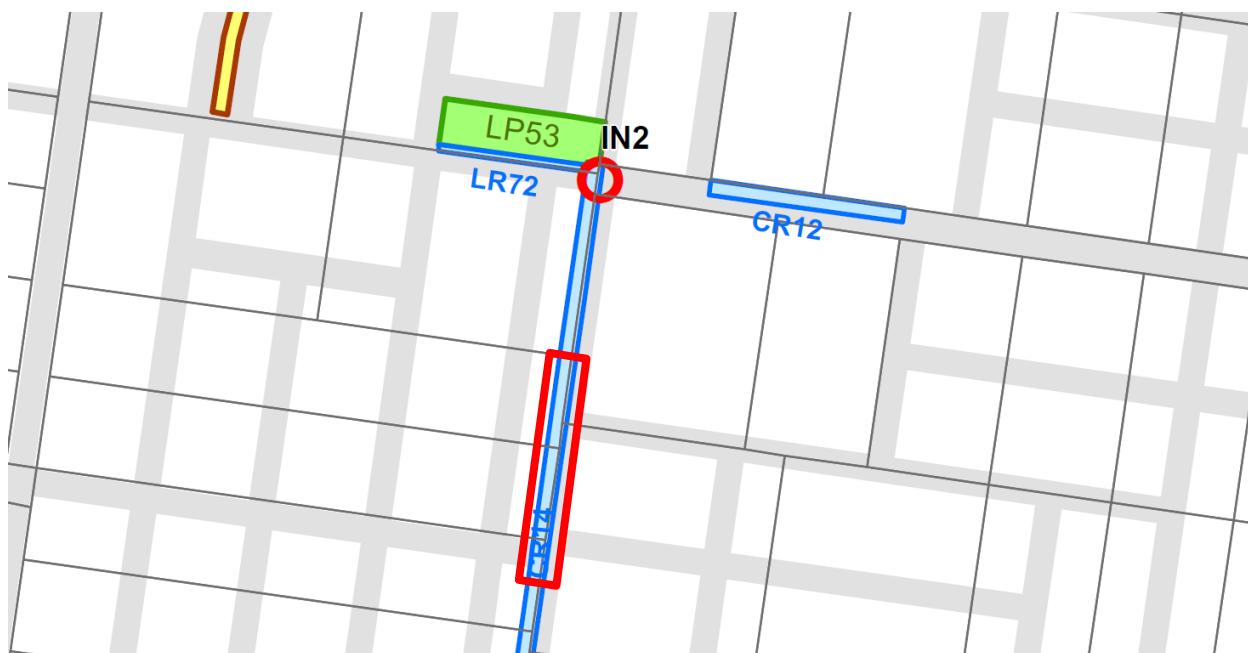
#### 4.13 WORKS-IN-KIND

As described, this application includes public domain works to the south-west of the subject site within the Council owned allotment known as Lot 664 in DP 1260834, and will form part of a Works-in-Kind Agreement. These works are located within the western half of the Warrawal Avenue, for the length of Lot 664, and also between the connection point of Little Street and Warrawal Avenue as shown in **Figure 14** below. The proposed public domain works will enable pedestrian and vehicular access from the established residential dwellings on the western side of Nemean Road, to the subject site. In the short to medium term, these works will improve viability of the proposed development, whilst maximising accessible for surrounding residents.



**Figure 14** Council owned Lot 664 in DP 1260834 edged in red, connection point between Little Street and Warrawal Avenue in blue, and subject site marked with a Star.

Within the *Liverpool Contributions Plan 2014 – Austral and Leppington North Precincts* ('Contributions Plan'), *Part 5 Works Schedules and Map*, notes that the land shown above is identified as a contribution area, 'Road Frontage Funding', CR14. This is shown in the *Contributions Plan Map Austral and Leppington*, which is reproduced in **Figure 15** below. These public domain works are to form part of the *Transport Management Facilities, Land for new Collector Roads*, within the Contributions Plan. The proposed works are consistent with the SP2 Local Road zoning per the Western Parkland City SEPP.



**Figure 15** Excerpt from Contributions Plan, Austral and Leppington North Map, with approximate location of public domain works edged in red

The proposal therefore seeks to undertake works consistent with CR14, which will facilitate a 'new road half width'. Of relevance, the contribution works for CR14 are for a total length of 495m and cost of approximately \$1,137,522, identified as a *Staging Priority 1* per the Contributions Plan. The proposal will only seek to undertake works for part of CR14, as described above, which will equate to approximately 123m. A QS Report submitted with this application indicates that the cost of works will equate to \$917,615.00.

These works are consistent with Council's *Development Contributions Land Dedication and Works-In-Kind Policy* (dated 27 February 2019). The proposed public domain works (set out in further detail below) will satisfy the provisions of the Works-in-Kind Policy (WIK Policy), as the works will offer a material public benefit and are identified in the Contributions Plan. It is anticipated that these works will be provided in exchange for a 'credit' or offset against Development Contributions. The Applicant and Council met during April 2023 to discuss the proposed WIK Agreement, in which Council were supportive. Owner's consent from Council has been submitted with this application, alongside the required documentation stipulated under Section 5.2 of the WIK Policy.

Following the above, the proposed public domain works are shown in the Civil Documentation prepared by *Henry & Hymas*. DA\_C100 to DA\_C103 of the civil drawings depict the extent of works proposed which align with the Contributions Plan and WIK Policy. These works will include the construction of roadway, kerbing, footpath, drainage and any associated landscaping. All works will be constructed in accordance with the relevant Australian Standards, as outlined in the supporting documentation submitted with this application.

This WIK Agreement will progress as part of the development application assessment and the works imposed as a condition of consent. It is requested that the timeframe for construction be discussed with Council and the proposed works be provided in exchange for a credit or offset against Development Contributions.

## 4.14 PROJECT DATA

The project data for the proposed development is provided in the table below.

Table 2 Project Data	
Site Area	Total: 18,170m <sup>2</sup> (1.817ha). Excluding roadways: 15,738m <sup>2</sup>
Gross floor area	Total GFA: 7,775m <sup>2</sup> .
FSR	Total floor space ratio of 0.42:1, noting no development standard applies.
Anchor Supermarket	One anchor supermarket with an area of 3,881m <sup>2</sup> , plus 200m <sup>2</sup> liquor tenancy.
Retail Area	1,521m <sup>2</sup>
Commercial Area	1,858m <sup>2</sup>
Building Height	Maximum building height of 13.5m to the edge of the parapet fronting Eighth Avenue
Car Parking	Total of 303 parking spaces, including 7 accessible spaces, 4 electric vehicle charging spaces and 6 direct to boot spaces. The proposal will also provide for 10 on-street parking spaces along Eighth Avenue and 6 spaces along Warrawal Avenue.
Motorcycle Parking	4 motorcycle spaces are proposed.
Bicycle Parking	24 bicycle spaces are proposed.
Loading Dock	19m heavy rigid vehicles.

## 5. Environmental Planning Assessment

### 5.1 PREAMBLE

This section of the Statement provides a planning assessment of the proposed development covering all relevant heads of consideration under Section 4.15 of the EP&A Act, 1979.

### 5.2 STATUTORY AND POLICY COMPLIANCE

The relevant matters for consideration under Section 4.15(1)(a) of the EP&A Act, 1979, are identified in **Table 3**.

Table 3 Section 4.15 Matters for Consideration				
EP & A Act, 1979.	Matters for Consideration	OK	See Comments	N/A
S.4.15(1)(a)(i)	SEPP Resilience and Hazards 2021	✓	✓	
"	SEPP (Transport and Infrastructure) 2021	✓	✓	
"	SEPP (Biodiversity and Conservation) 2021	✓	✓	
"	SEPP (Industry and Employment) 2021	✓	✓	
"	SEPP (Western Parkland City) 2021	✓	✓	
S.4.15(1)(a)(iii)	Liverpool Growth Centre Precincts Development Control Plan	✓	✓	
"	Liverpool Development Control Plan 2008	✓	✓	
S.4.15(1)(a)(iv)	Any other prescribed matter: – AS 2601-1991: Demolition of structures.	✓		

The matters identified in the above Table as requiring specific comment are discussed below. The primary statutory documents that relate to the subject site and the proposed development are *State Environmental Planning Policy (Western Parkland City) 2021* (Western Parkland City SEPP). The primary non-statutory plans relating to the subject site and proposed development is the *Liverpool Growth Centre Precincts Development Control Plan* (LGCP DCP) and *Liverpool Development Control Plan 2008* (LDCP 2008). An assessment of the applicable provisions of these documents and other relevant planning instruments is provided below.

#### 5.2.1 SEPP Resilience and Hazards 2021

*State Environmental Planning Policy (Resilience and Hazards) 2021* (Resilience and Hazards SEPP) commenced on 1 March 2022, repealing and replacing three former SEPPs related to coastal management, hazardous and offensive development and remediation of land. Of relevance to the proposed development is *Chapter 4 Remediation of Land*.

Chapter 4 of the Resilience and Hazards SEPP 2021 provides planning controls for the remediation of contaminated land and requires an investigation to be made if land contamination is suspected. The site has been utilised for residential purposes for a considerable period of time, however, a Detailed Site Investigation (DSI) prepared by Geo-Logix has been submitted with this application. The DSI has concluded the following:



*‘...Results of the investigation identified a non-conformance of the project decision rules due to fragments of bonded ACM and AF/FA (associated with weathered ACM) in shallow soils in the central portion of the northern half of the site (surrounding TP15 and TP50).*

*On this basis the project decision has not been met and the site is not considered suitable for the proposed retail development without the requirement for minor soils remediation. Geo-Logix’s opinion is that the site can be made suitable for the proposed development subject to the following:*

- Preparation of a Remedial Action Plan (RAP) detailing the remedial methodology and validation requirements for asbestos impacted soils; and*
- Remediation and validation of a minor area of asbestos impacted soils.*

*It is recommended site demolition be conducted prior to remediation in the event the demolition works results in localised residual hazardous building material contamination to shallow soils...’*

Per the above, minor contaminants have been found on the subject site as set out in the DSI. To make the site suitable for the proposed development, minor remediation works will be required which will form part of a Remedial Action Plan, which can imposed as a condition of consent. Following remediation, the site will be suitable for the proposed use as determined by *Geo-Logix*.

As such, the proposal is considered acceptable with regards to Chapter 4 of the Resilience and Hazards SEPP 2021, following the recommendations of *Geo-Logix*.

### **5.2.2 State Environmental Planning Policy (Transport and Infrastructure) 2021**

*State Environmental Planning Policy (Transport and Infrastructure) 2021* commenced on 1 March 2022, repealing and replacing four former SEPPs related to infrastructure, transport, education and childcare. Of relevance to the proposed development is *Chapter 2 Infrastructure*.

*Clause 2.122 Traffic Generating Development* of Chapter 2 applies to the proposed development as it includes a car park which has more than 200 car parking spaces, with access to any road. As such, pursuant to Schedule 3 of the Transport and Infrastructure SEPP, the proposed development is classified as traffic-generating development.

The consent authority is therefore required to refer the development application to Transport for NSW (formerly RMS) and take into consideration any comments received from the referral, as well as the accessibility of the site and any potential traffic safety, road congestion or parking implications of the development.

A Traffic and Parking Impact Assessment has been prepared by *Colston Budd Rogers & Kafes* and accompanies the application.

### **5.2.3 SEPP (Biodiversity and Conservation) 2021**

*State Environmental Planning Policy (Biodiversity and Conservation) 2021* (Biodiversity and Conservation SEPP) commenced on 1 March 2022, repealing and replacing 11 previous SEPPs. Of relevance to the proposed development is *Chapter 2 Vegetation in non-rural areas*.

*Chapter 2 Vegetation in non-rural areas* replaces the previous SEPP (Vegetation in Non-Rural Areas) 2017. The primary aims and objectives of Chapter 2 are related to the protection of the biodiversity values of the trees and other vegetation on the site in non-rural areas. This chapter works together with the *Biodiversity Conservation Act 2016* and the *Local Land Services Amendment Act 2016* to create a framework for the regulation of clearing of native vegetation in NSW.

Chapter 2 regulates clearing that is not ancillary to development requiring consent. Whereas, clearing that is ancillary to development requiring consent will be assessed as part of the development assessment process. As such, this chapter is not applicable to the proposed development as the removal of any trees would be ancillary to development requiring consent. Of relevance to this application, a number of trees are located on the subject site which will be removed and replaced according to the submitted Landscape Plan. An Arborist Report prepared by *Naturally Trees* is also submitted with this application and has assessed the impacts of the proposal on the existing vegetation.

#### 5.2.4 SEPP (Industry and Employment) 2021

*State Environmental Planning Policy (Industry and Employment) 2021* (Industry and Employment SEPP) commenced on 1 March 2022, repealing and replacing two former SEPPs related to employment lands in Western Sydney and advertising and signage. Of relevance to the proposed development is *Chapter 3 Advertising and Signage*.

Chapter 3 applies to all signage that can be displayed with or without development consent and is visible from any public space. The proposal includes the provision of numerous signs, including the following:

- Centre identification signage which states 'Austral South';
- Business identification signage for the proposed supermarket and liquor tenancy, which states 'Woolworths' and 'BWS', respectively; and
- Wayfinding and operational signage, including 'Direct-to-boot', 'Parking', 'Loading Bay' and 'Trolley Bay'.

These signs will include external façade signs, suspended signs, canopy signs, pylon signs and cantilevered pylon signs, as shown in **Figure 16** below. The signs are located on the northern, western and southern facades of the building, in addition to the at-grade parking area. Whilst signage for the specialty retail and commercial tenancies is not proposed, the location and size of these signs is demonstrated on the architectural set. For example, future tenancy signage will be provided on the main pylon sign, S1.1, S5.1 and S5.2, per the below.

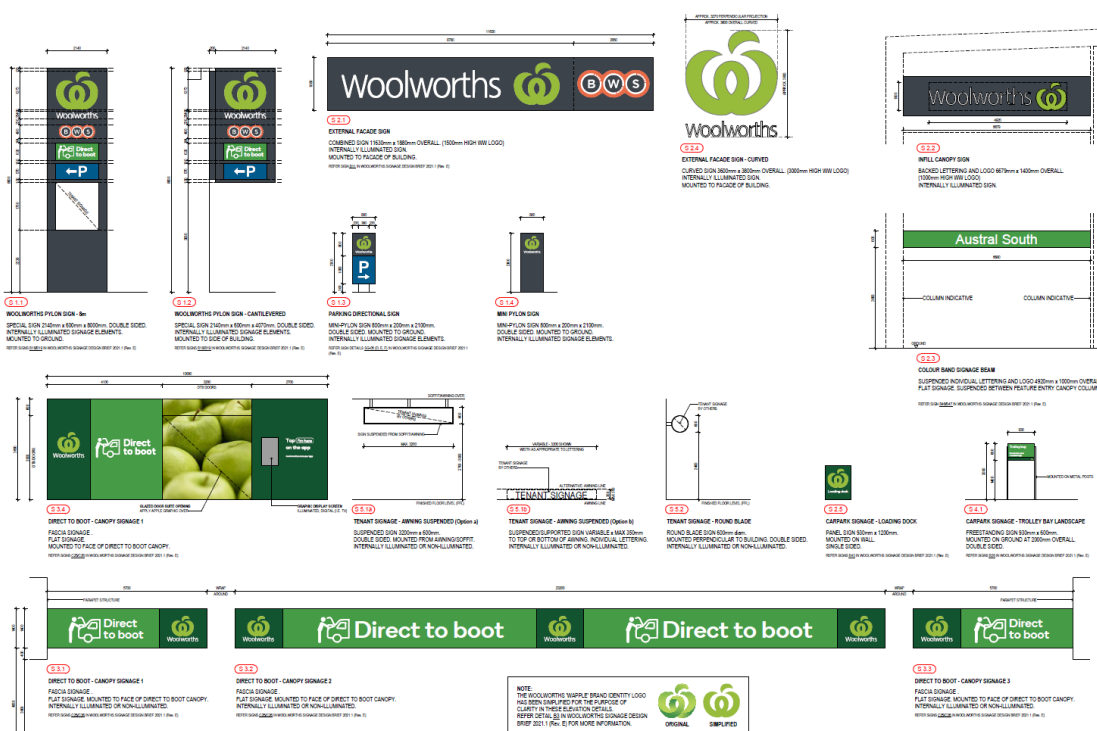


Figure 16 Proposed signage

The relevant objectives set out in clause 3.1(1)(a) and the applicable assessment criteria specified in Schedule 5 of the Instrument have been addressed below in **Table 4**.

<b>Table 4 Chapter 3 Advertising and Signage Assessment</b>		
Criteria	Requirement	Discussion
3.1(1)(a)	<p>(a) to ensure that signage (including advertising):</p> <p>(i) is compatible with the desired amenity and visual character of an area, and</p> <p>(ii) provides effective communication in suitable locations, and</p> <p>(iii) is of high quality design and finish</p>	<p>The proposal includes centre and business identification signage for Woolworths and BWS, located across the development as shown on the architectural plans. Location and size of signage is also provided for the other commercial and retail tenancies.</p> <p>The signage is simple in design, materiality and character and will not be visually obtrusive from the public domain. The signs will be compatible with the commercial character desired on the subject site and will complement the neighbouring B1 land to the east. The proposed signage is also sympathetic in scale and design which ensures that the surrounding residential zones will not be impacted.</p> <p>The signage will be clear and legible as viewed from the public domain and is suitably located to ensure visibility from the public domain. The signage also includes wayfinding and operational signs, which will assist the general public in identify and circulating through the site. It is also noted that the signage will not extend beyond the maximum building height of the built form.</p> <p>The signage will be of a contemporary design and will utilise prefabricated materials for a high quality finish which will not be obtrusive when viewed from the public domain. Signage will also be internally illuminated. This will be consistent with the overall character and design of the proposed centre.</p>
1 Character of the area	<p>• Is the proposal compatible with the existing or desired future character of the area or locality in which it is proposed to be located?</p>	<p>The signage is contemporary in appearance, materiality and is well coordinated. The proposed signage will be located strategically throughout the development, including affixed signage, suspended signage, pylon signage and the like. This will be limited in its overall size and scale.</p> <p>In addition, the proposed signs are compatible with the land use envisaged in the zone, including the existing buildings and desired future character of the locality.</p>
	<p>• Is the proposal consistent with a particular theme for outdoor advertising in the area or locality?</p>	<p>The locality of the area does not identify a particular theme for signage and is anticipated to undergo change given the permitted uses on the subject site. The proposed signs are modest in scale and are compatible with the contemporary</p>

**Table 4 Chapter 3 Advertising and Signage Assessment**

		built form proposed. Whilst the signs are contemporary, they are simple in design and materiality and will not be visually intrusive as identified through this analysis.
2 Special areas	<ul style="list-style-type: none"> <li>• Does the proposal detract from the amenity or visual quality of any environmentally sensitive areas, heritage areas, natural or other conservation areas, open space areas, waterways, rural landscapes or residential areas?</li> </ul>	The subject site is not situated within an environmentally sensitive area, heritage conservation area, open space area, or the like. The site is located within the B1 Neighbourhood Centre and is anticipated to be redeveloped in line with the permitted controls. In this regard, the signage is proposed so that it is modest in design and will not overbear with regards to size, scale and materials the proposed built form and surrounding residential zones.
3 Views and vistas	<ul style="list-style-type: none"> <li>• Does the proposal obscure or compromise important views?</li> </ul>	There are no significant views obtained from or through the site. The signage will not contribute to the impediment of any views.
	<ul style="list-style-type: none"> <li>• Does the proposal dominate the skyline and reduce the quality of vistas?</li> </ul>	The affixed signage will be situated below the upper most edges of the roof. Furthermore, the signage is also considerably lesser in size and scale than the overall built form and as such, the signage will not impact the skyline in any significant way.
	<ul style="list-style-type: none"> <li>• Does the proposal respect the viewing rights of other advertisers?</li> </ul>	The proposed signage will not comprise the viewing rights of any future advertisers on the neighbouring B1 zone or within the surrounding locality.
4 Streetscape, setting or landscape	<ul style="list-style-type: none"> <li>• Is the scale, proportion and form of the proposal appropriate for the streetscape, setting or landscape?</li> </ul>	The proposed signage on the built form and at-grade, fronting the public domain, is compatible with the desired future character of the locality. The proposed signage will complement the contemporary façades through the proposed colours and style. As detailed, the mixture of signage will be commensurate to the overall scale of the site and proposed built form. This will be appropriately located throughout the site and will contribute to the overall cohesiveness of the centre. It is also noted that the signage will not impact any established or future landscaped character.
	<ul style="list-style-type: none"> <li>• Does the proposal contribute to the visual interest of the streetscape, setting or landscape?</li> </ul>	The proposed signage, which is contemporary in design and materiality, has been designed with intent to integrate into the overall built form and provide a degree of visual interest whilst enhancing the streetscape appearance of the centre. The signage is strategically placed and oriented as to improve visual interaction.



**Table 4 Chapter 3 Advertising and Signage Assessment**

	<ul style="list-style-type: none"> <li>• Does the proposal reduce clutter by rationalising and simplifying existing advertising?</li> </ul>	<p>There is no existing advertising on the subject site. However, the proposed signage scheme has been designed and is compatible with the overall built form and character of the site. The design of all signs are simple and do not create any visual impact to the surroundings. The proposed signage provides a rational and effective means of communication and will not adversely impact streetscape character.</p>
	<ul style="list-style-type: none"> <li>• Does the proposal screen unsightliness?</li> </ul>	<p>The proposed signage will complement the overall appearance of the development, which is contemporary in nature. The proposed signage provides visual interest on the numerous facades and at-grade, and will not appear as unsightly or obtrusive. The proposed materiality is also simplistic, contemporary and complementary to the overall design.</p>
	<ul style="list-style-type: none"> <li>• Does the proposal protrude above buildings, structures or tree canopies in the area or locality?</li> </ul>	<p>The proposed signage will not extend beyond the highest roof structure of the built form. This includes both the proposed affixed and pylon signage.</p>
	<ul style="list-style-type: none"> <li>• Does the proposal require ongoing vegetation management?</li> </ul>	<p>The proposed signage will not require any ongoing vegetation management.</p>
5 Site and building	<ul style="list-style-type: none"> <li>• Is the proposal compatible with the scale, proportion and other characteristics of the site or building, or both, on which the proposed signage is to be located?</li> </ul>	<p>The signage is designed to complement with rather than overwhelm the development. That is, the centre, business identification and wayfinding signage is modest in scale and utilises a contemporary design and materiality which will contribute to the overall character of the building. When viewing the development as a whole, the signage will not compete with the overall built form and will appear as subordinate.</p>
	<ul style="list-style-type: none"> <li>• Does the proposal respect important features of the site or building, or both?</li> </ul>	<p>The signage scheme, in combination with the building façades, will enhance the aesthetic qualities of the building. The overall character of the development is contemporary which is maintained in the proposed signage. The signage will add to the visual interest of the development. It is noted that there are no important features of the site which need to be considered.</p>
	<ul style="list-style-type: none"> <li>• Does the proposal show innovation and imagination in its relationship to the site or building, or both?</li> </ul>	<p>The signage scheme is of a contemporary design and utilises emerging technologies to fabricate lettering and symbols for a high-quality finish. Appropriate illumination and in some instances graphic display signs (S3.4) are provided, showing innovation. Overall, the signage will complement the contemporary character of the development.</p>

**Table 4 Chapter 3 Advertising and Signage Assessment**

6 Associated devices and logos with advertisements and advertising structures	<ul style="list-style-type: none"> <li>• Have any safety devices, platforms, lighting devices or logos been designed as an integral part of the signage or structure on which it is to be displayed?</li> </ul>	Lighting has been designed as part of all signage, being illuminated internally. Safety devices in the sense of scaffolding and rails are not required for the proposed signage.
7 Illumination	<ul style="list-style-type: none"> <li>• Would illumination result in unacceptable glare?</li> </ul>	Signage will be internally lit and will not result in unacceptable glare.
	<ul style="list-style-type: none"> <li>• Would illumination affect safety for pedestrians, vehicles or aircraft?</li> </ul>	Illumination is specifically directed and designed so as not to cast light beyond the signage it is designed to illuminate. In any case, illumination levels are not likely to affect the safety of pedestrians, vehicles or aircraft.
	<ul style="list-style-type: none"> <li>• Would illumination detract from the amenity of any residence or other form of accommodation?</li> </ul>	As above, illumination is internal and will not result in light spill. Where dwellings oppose the subject site, the signage will be appropriately setback and will not be illuminated to an undesirable level. This will ensure that the amenity of existing and future residential dwellings will be unaffected.
	<ul style="list-style-type: none"> <li>• Can the intensity of the illumination be adjusted, if necessary?</li> </ul>	The intensity of illumination is not proposed to be adjustable however will have a moderate intensity in any case. If considered necessary, the consent authority may choose to impose a condition of consent requiring the installation of lighting with adjustable illumination levels.
	<ul style="list-style-type: none"> <li>• Is the illumination subject to a curfew?</li> </ul>	The lighting is not proposed to be extinguished overnight, however, will have a moderate intensity. If considered necessary, the consent authority may choose to impose a condition of consent requiring that illumination is extinguished during certain hours, although it would be appreciated if curfew hours were discussed with the applicant before being imposed.
8 Safety	<ul style="list-style-type: none"> <li>• Would the proposal reduce the safety for any public road?</li> </ul>	The proposed signage is modest in terms of scale, design and illumination will not comprise the safety of the adjacent public roads.
	<ul style="list-style-type: none"> <li>• Would the proposal reduce the safety for pedestrians or bicyclists?</li> </ul>	As above, the proposed signage scheme is not likely to compromise safety of pedestrians or cyclists either through causing a distraction or a physical obstruction.
	<ul style="list-style-type: none"> <li>• Would the proposal reduce the safety for pedestrians, particularly children, by obscuring sightlines from public areas?</li> </ul>	The proposed signage will have no impact to the sightlines for pedestrians (including children) or vehicles.

Accordingly, the proposal will satisfy *Chapter 3 Advertising and Signage* of the Industry and Employment SEPP.

### 5.2.5 State Environmental Planning Policy (Precincts – Western Parkland City) 2021

*State Environmental Planning Policy (Precincts – Western Parkland City) 2021* (Western Parkland City SEPP) commenced on 1 March 2022, repealing and replacing six former SEPPs related to land located within the Western Parkland City region identified in the *Greater Sydney Region Plan – A Metropolis of Three Cities*. Of relevance to the proposed development is *Chapter 3 Sydney Region Growth Centres*.

Chapter 3 regulates land uses within the South West Growth Centres, specifically, the Austral Precinct as it applies to the subject site. The relevant planning controls for the site are contained in *Appendix 4 - Liverpool Growth Centres Precinct Plan* of the Western Parkland City SEPP. The applicable controls are outlined below and within **Annexure A**, and where required, the proposal is assessed against them.

Under the SEPP, Appendix 4, the subject site is within *Zone B1 Neighbourhood Centre*, as indicated on the Land Zoning Map. The proposed development is characterised as a commercial development which contains the following uses, as are permissible in the zone:

- Supermarket (defined as a *shop*) as is permitted with consent under 3;
- Business and Office premises, as is permitted with consent under 3; and
- Specialty Tenancies, which are capable of accommodating Shops, Medical Centres, Neighbourhood Shops (where less than 100m<sup>2</sup>) and veterinary hospitals, as is permitted with consent under 3.

In accordance with the above, fit-out and use of the abovementioned specialty tenancies and supermarket will form part of separate applications, per Section 4.6 of this Statement. The objective of the B1 Zone are as follows:

- *To provide a range of small-scale retail, business and community uses that serve the needs of people who live or work in the surrounding neighbourhood.*

The proposed development will meet the objective of the zone through the provision of a high-quality development which offers a variety of uses to meet the needs of the community, including those who live and work in the surrounding neighbourhood. The built form will present to the public domain as a contemporary development with substantial articulation, including the provision of a north-south through-site link and public piazza.

In addition to the above, the proposal will involve public domain works within the SP2 Infrastructure zone, as part of the Works-in-Kind Agreement. The proposed roadworks and services are permitted within the zone and will satisfy the objectives, which is to provide infrastructure in the locality.

A Compliance Table which considers the proposal against the provisions of Western Parkland City SEPP is provided at **Annexure A**.

### 5.2.6 Liverpool Growth Centre Precincts DCP

The *Liverpool Growth Centre Precincts Development Control Plan* (LGCP DCP) applies to the subject site. A Compliance Table is provided at **Annexure B** and considers the proposal against the relevant controls of the DCP, including precinct-specific controls for Austral and Leppington North Precincts (Schedule 1).

The compliance table demonstrates that the proposal is largely consistent with relevant LGCP DCP controls. Areas of non-compliance are discussed and justified within the compliance table and below. The proposed variations are considered acceptable in accordance with Section 4.15(3A) of the EP&A Act, which requires that DCP provision are to be applied flexibly, as follows:

(3A) *Development control plans* If a development control plan contains provisions that relate to the development that is the subject of a development application, the consent authority—

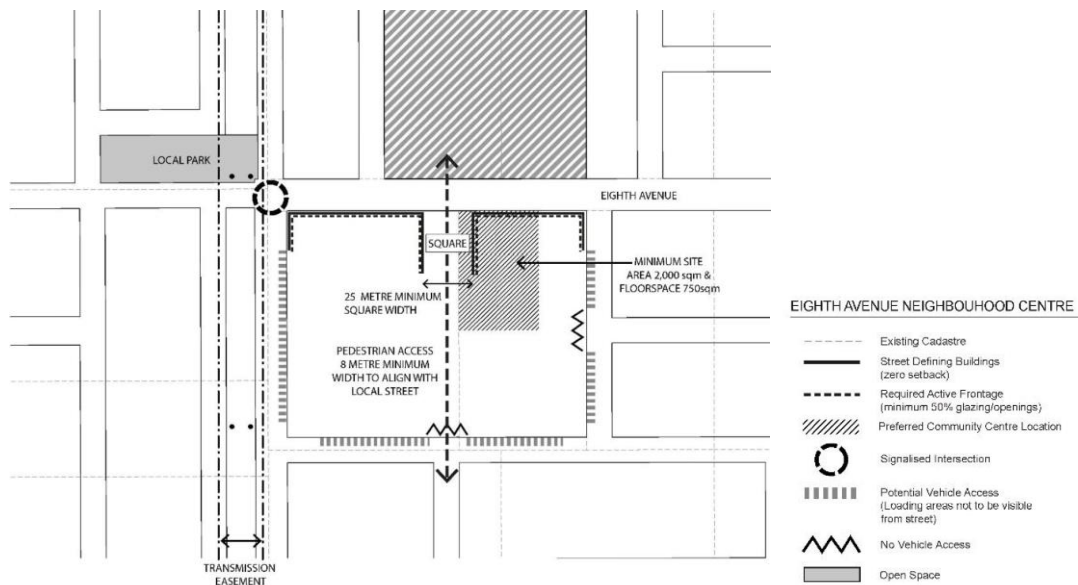
(a) *if those provisions set standards with respect to an aspect of the development and the development application complies with those standards—is not to require more onerous standards with respect to that aspect of the development, and*

(b) *if those provisions set standards with respect to an aspect of the development and the development application does not comply with those standards—is to be flexible in applying those provisions and allow reasonable alternative solutions that achieve the objects of those standards for dealing with that aspect of the development, and*

(c) *may consider those provisions only in connection with the assessment of that development application.*

### 5.2.7 Indicative Layout Plan

The proposed development is generally designed in accordance with the Indicative Layout Plan (ILP) prescribed by Section 3.1.4 *Eighth Avenue Neighbourhood Centre* the DCP. That is, the proposal will include the provision of a commercial development with a north-south through-site link and public piazza, as is desired by the ILP, thus satisfying the objectives of the zone. Whilst satisfying the intent of the ILP, the proposal will seek to vary the prescribed design, particularly as it pertains to the location of the north-south through-site link. The ILP as recommended by the DCP is shown in **Figure 17** below.



**Figure 17** Eighth Avenue Neighbourhood Centre Layout

As detailed, the proposal has relocated the north-south through site link entirely within the subject site and provided specialty retail tenancies along the eastern boundary shared with the neighbouring property. Whilst this differs from the ILP, it is noted that the subject site has a frontage to Eighth Avenue of 120m, whilst the property to the east, which is similarly zoned B1, a frontage of 80m. Due to the imbalance of frontage widths between the subject site and neighbouring property, the relocation of the through-site link by 10m to the west is considered to appropriately respond to the characteristics of the locality. This will deliver a through-site link which is located centrally within the B1 zoned

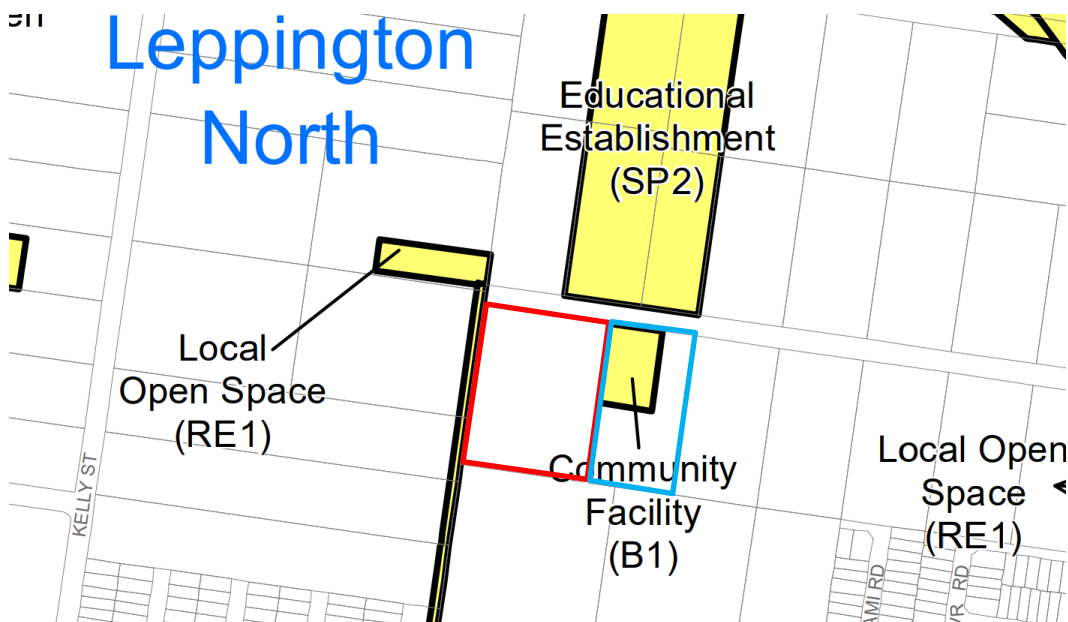


land, providing a 90m setback to the boundary shared with the western road, and 90m setback to the eastern boundary of the neighbour, No. 240 Eighth Avenue.

Whilst varying the ILP, the proposal is consistent with the objectives of Section 3.1.4 as reproduced below:

- a. To facilitate the development of neighbourhood centre retail, commercial and community uses that provide access to shopping, employment and services for residents in parts of the precincts between Bonds Creek, Kemps Creek and Bringelly Road.*
- b. To encourage a high standard of development and a quality, attractive environment within the neighbourhood centre.*

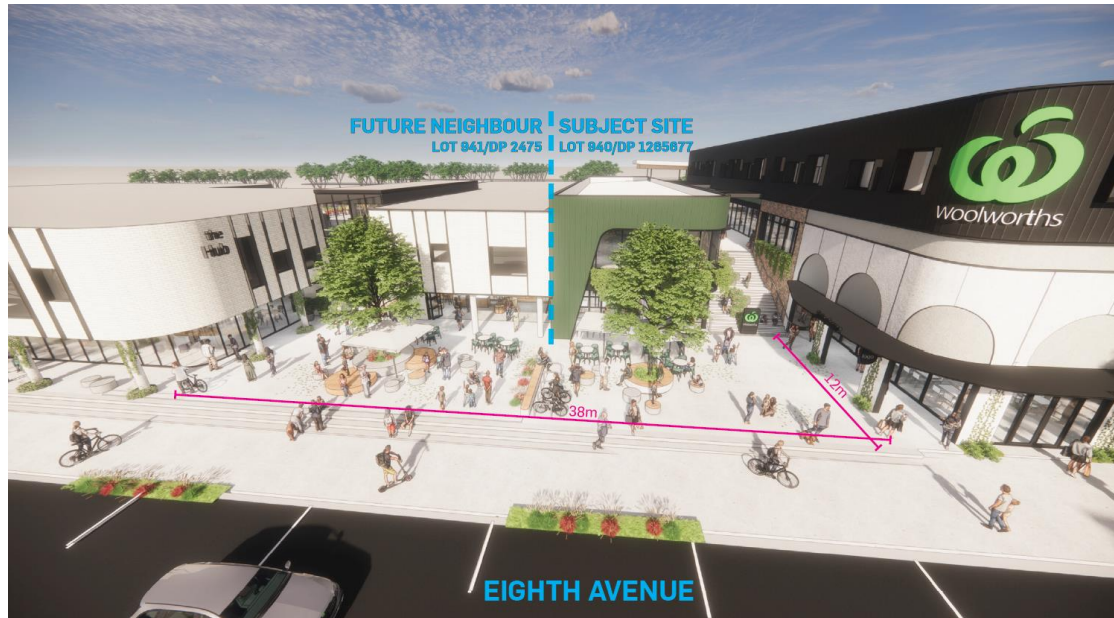
In accordance with the above, the proposal has considered the characteristics of the site and surrounding locality to inform the site planning and design. As detailed above, the through-site link is located centrally within the B1 zoned land and will provide direct pedestrian access from Eighth Avenue through to Auger Street, via a dedicated pathway provided through the public piazza, centre and at-grade car park. This through-site link, whilst being relocated slightly to the west, will be activated by a variety of uses, will be generously sized and partly open to the sky, as to encourage pedestrian movement. The relationship of the through-site link to the proposed anchor supermarket, numerous tenancies, public piazza and at-grade parking area will ensure its constant activation. Importantly, the neighbouring property is required to provide a significantly sized (B1) Community Facility in accordance with the Land Reservation Acquisition Map, per **Figure 18** below.



**Figure 18** Excerpt of Land Reservation Acquisition Map (Site edged in red, neighbour edged in blue)

As shown above, the neighbouring property must provide and dedicate a community facility to Council in the position indicated on the Land Reservation Acquisition Map. That is, the location and siting of this community facility must not be altered from that set out in the Map and under Clause 5.1 of Appendix 4 of Western Parkland City SEPP, and occupies half the length of the boundary shared with the subject site. As a result, it is considered to be a superior outcome to relocate the through-site link to the west and activate this space with the proposed anchor supermarket, specialty tenancies and kiosks, as a community facility will not offer the same level of activation and pedestrian movement as is proposed. This is also considered to provide a greater safety benefit.

Following the above, whilst the proposed through-site link will not straddle the boundary shared with the neighbouring property, it will not prejudice the redevelopment of this neighbour, as shown in **Figure 19** below.



**Figure 19** Potential built form of neighbouring property to the east, from Eighth Avenue

The above image indicates that whilst the through-site link is relocated, it will not impact the redevelopment potential of the neighbouring property. The proposed public piazza, which seeks to provide a wider frontage to the public domain, will ensure that the neighbouring property can be redeveloped with a high level of activation, whilst improving amenity and safety, allowing users and general public to equally access both sites. Furthermore, the future community facility and any non-residential and residential uses will be supported by the proposed development, with access maintained along Eighth Avenue and to the south of the proposed built form.

In this regard, the proposed development and potential redevelopment of the neighbouring property will deliver a high quality and attractive neighbourhood centre, despite the proposed ILP amendments. The proposal will deliver for a range of uses which will activate the locality and neighbourhood centre. The relocation of the through-site link will not negatively impact quality of the B1 zone and will encourage pedestrian movement from Eighth Avenue to Auger Street. This is achieved through the design of the public piazza and through-site link, including the extensive activation from the proposed uses. Where the through-site link traverses the at-grade parking area, it will be clearly identified and embellished by landscaping.

As such, the proposed site and building arrangement is considered to satisfy the objectives of the Eighth Avenue Neighbourhood Centre and the location of the north-south through site link appropriate.

### 5.2.8 Active Street Frontages

As indicated in **Figure 17** and per *Section 3.1.4 Eighth Avenue Neighbourhood Centre*, active street frontages are required along Eighth Avenue, the north-western corner of the site, the public piazza and part of the through-site link. Whilst the proposal predominantly satisfies the ILP as it pertains to the provision of active street frontages, it will not activate the north-western corner of the site, where Eighth Avenue and Warrawal Avenue intersect. The proposed variation is a direct result of the site topography, which results in the lower ground floor in the north-western corner of the site being below natural ground. As the finished floor level of the piazza is set by the level of Eighth Avenue and



the upper ground floor set by the relationship of the site to Auger Street, the provision of additional active frontages along the north-western corner is not possible.

In lieu of the above, the proposal will offset the minor variation through the provision of extensive frontages within the public piazza, throughout the through-site link and along the southern façade, where it opposes the at-grade carpark and Auger Street. This is proposed as to maximise the activation of the neighbourhood centre at multiple frontages and within the highly pedestrianised areas. The relocation of the active frontages beyond that envisaged by the ILP is therefore considered to be a superior outcome, as it will substantially improve the activation through-site link and as the development is orientated to the south, whilst still maintaining appropriate activation of Eighth Avenue.

Importantly, the proposed location of active frontages will satisfy the intent of the ILP, whilst providing additional activation beyond that envisaged for the subject site. Whilst the north-western corner of the site opposes the RE1 zone, it is located at the juncture of Eighth Avenue and Warrawal Avenue, which are both identified as collector roads. Resultantly, providing active frontages at the north-western corner of the site will not bring any discernible benefit to the locality, due to its vehicular nature. The relocation of active frontages is therefore considered to result in wholesale improvements to the pedestrianised character envisaged for the site. It is also imperative to note that the proposal will require significant redesign to activate and provide at-grade access to the north-western corner of the site. This will result in convoluted floor levels, circulation and building design, ultimately impacting accessibility and overall presentation to the streetscape, which would result in an inferior outcome.

To also offset this variation, the north-western corner will be articulated with an artwork which will add visual interest. Therefore, the proposed development will deliver an appropriate balance of active street frontages and uses contained within a high-quality contemporary form, as required by the objectives of *Section 3.1.4 Eighth Avenue Neighbourhood Centre*.

#### **5.2.9 Liverpool DCP 2008**

The *Liverpool Development Control Plan 2008* (LDCP 2008) applies to the subject site where the LGCDP does not apply. This primarily pertains to technical detailing and has been considered throughout the relevant documentation submitted with this application. As such, any requirements which form part of the LDCP are considered to be reasonably satisfied.

### **5.3 IMPACTS ON NATURAL & BUILT ENVIRONMENT**

#### **5.3.1 Topography & Scenic Impacts**

The proposal requires excavation to accommodate the at-grade on-site parking, the built form and public domain works. The proposed development is designed to respond to the topography of the site to minimise any visual or physical impact, as viewed from the public domain or surrounding properties. The completed development will occupy the extent of excavated area and is necessary for the efficient use of the site.

As detailed, it is noted that a degree of excavation and modification to the landform is required given the undulating topography. This is required to deliver active frontages, pedestrian and vehicle access and an attractive built form. This results in unavoidable excavation which will also accommodate the active uses at natural ground, responding to the topography of the site. To protect the character of the locality, the topography around the periphery of the site has been maintained. This has been integrated with active frontages, the public piazza and a pedestrian through-site link. Furthermore, the proposal will provide a proportionate quantum of landscaped area to soften the built form and maximise permeability.

Where the excavation is located in close proximity to neighbouring properties and the public domain, appropriate measures will be undertaken in accordance with the recommendations provided in the Geotechnical Assessment.



Dilapidation Reports are also anticipated to be imposed as a condition of consent to ensure the protection of the neighbouring properties during excavation. The Geotechnical Assessment has also considered salinity, which based on laboratory testing, is not an issue for the subject site.

In terms of scenic impacts, the proposal is considered to be consistent with the desired future character of the area and changing density of the locality. The proposed excavation and building design will not adversely impact the amenity of neighbouring sites or the structural integrity of nearby buildings and public domain and is considered acceptable.

### 5.3.2 External Appearance & Design

The proposed development represents an excellent response to the site constraints and reasonable permitted density on the site. The proposal, being a contemporary commercial development with a defined base and articulated upper levels, is consistent with the desired built form anticipated by the applicable planning controls and objectives of the B1 zone. The architectural language utilises a high quality, contemporary design to ensure the development will reflect the neighbourhood character of the site and provide a gateway building within the immediate locality, thus achieving a high level of visual interest.

The proposal is designed with a defined base and recessed upper levels as it addresses Eighth Avenue, parts of Warawal Avenue and the through-site link. As the built form addresses the remaining frontages and at-grade parking area, it is predominately single storey in height. Throughout both the external and internal facades, the proposal provides active frontages at natural ground with articulated upper levels. These facades incorporate a mixture of curved and rectilinear forms, horizontal and vertical articulation and fenestration. The proposal utilises this simplified and well-considered architectural language to create an inviting pedestrian environment. The defined base will be finished in concrete, brickwork and glazing, with the proposed upper levels finished in a visually lightweight metal cladding. Importantly, the proposed base and upper levels have responded to the steep topography of the site to provide a built form which steps to reduce the visual bulk and scale of the development. This is incorporated with the through site link, public piazza and at-grade parking area, as to reduce the visual bulk and scale of the development.

The proposal will also include artwork located on the north-western corner of the built form, which will form part of a condition of consent and will provide a high level of visual interest and articulation.

Photomontages of the proposed development are provided in **Figures 20** and **21** below.



**Figure 20** Perspective of proposed development from Eighth Avenue, as viewed from public piazza





**Figure 21** Perspective of proposed development from on-site parking area

The internal design and layout of the proposed centre will enable ease of pedestrian circulation, safety and high levels of amenity, including solar access and ventilation. The proposed through-site link and public piazza are easily identifiable from the street frontages and will enable a clear path of travel. Integral to this is also the provision of high quality landscaping throughout the site which provides a visual and physical relationship the built form and surrounding residential character. The proposal has concentrated landscaping within the public piazza, through-site link and at-grade parking area, inclusive of shrubs, mature canopy trees and vertical vegetation. This is further bolstered through the provision of additional street tree planting located within the public domain.

Accordingly, the proposed development will provide a high quality architectural design which is compatible with the locality.

### 5.3.3 Solar Access

There are no specific controls within the DCP that deal with solar access to neighbouring properties under the B1 Neighbourhood Centre zone. As such, the solar amenity of neighbouring properties surrounding the subject site have been considered on merit. It is important to note that the site orientation and permitted increase of density will result in a degree unavoidable overshadowing to the surrounding properties and public domain. Notwithstanding, the proposal is designed to maintain solar access as far as practicable, as the built form is generally limited in footprint and height.

The following is noted regarding the surrounding properties;

- **Eastern neighbour;** To the east is No. 240 Eighth Avenue, which is similarly zoned as B1 Neighbourhood Centre. A minor portion of this property will be overshadowed by the proposal for approximately 3 hours during mid-winter, from 12pm to 3pm, therefore maintaining solar access for 3 hours. This is considered appropriate if the neighbouring property is to include shop top housing, as an ample amount of solar access during mid-winter will be retained;
- **Properties to west:** To the west of the subject site and on the opposite side of Warrawal Avenue is R3 Medium Density Residential zoned land. The proposed development will result in overshadowing for 1 hour from 9am to 10am during midwinter and will therefore retain 5 hours of solar access and is considered acceptable; and
- **Properties to south:** Due to the location of the at-grade car park, there will be no additional overshadowing to any future residents on the southern side of Auger Street.

In terms of the proposed public piazza and surrounding public domain, the following is required per the DCP;

- '...1. Parks and plazas are to receive sunlight on a minimum of 50% of their site area between 11am and 2pm on June 21st.*
- 2. Building envelopes are to allow for north-south streets to receive 2 hours sunlight between 9am-3pm on June 21st on a minimum of 50% of the eastern or western footpaths; and*
- 3. Building envelopes are to allow for east-west streets to receive 2 hours of sunlight between 9am-3pm on June 21st on a minimum of 50% of the southern footpaths...'*

With regards to the proposed public piazza, this is orientated to the north and will also retain solar access to 100% of this space between 9am and 3pm on 21 June and is considered acceptable. In terms of solar access to the streets, given the site orientation and anticipated increase of density, there will be unavoidable impact. To Warrawal Avenue, the proposal will retain 3 hours of solar access to 100% (or greater) of the pedestrian footpath along the boundary shared with the site and is acceptable. To both Eighth Avenue and Auger Street, the proposal will not result in any additional overshadowing to the public domain due to orientation and separation distances, respectively.

As such, the extent of overshadowing is reasonable and as anticipated by the site orientation, topography and building envelope controls that apply to the site.

#### **5.3.4 Aural & Visual Privacy**

In terms of privacy, the proposed development has been designed to minimise, as far as practicable, the likelihood of any adverse overlooking or invasion of aural privacy of neighbouring properties. The proposal provides for a commercial development with publicly accessible spaces and has utilised setbacks, orientation and blank facades to limit any privacy impacts to existing and future neighbouring properties.

To future residential accommodation surrounding the subject site, the proposal is separated by the width of the roadways. This is considered to mitigate any opportunity for visual privacy impacts to future residents, achieved by virtue of separation, the commercial nature of the development and orientation of views to the front setback areas of neighbouring properties. To Eighth Avenue, whilst openings are proposed from active frontages on the lower ground floor and commercial tenancies on level 1, the separation between the future residents at No. 355 Eighth Avenue to the north-west limits any impacts. Further to this, the majority of openings oppose the SP2 Educational Establishment zoned land, which ultimately mitigates any privacy concerns.

To the eastern (side) boundary, the proposal provides a nil setback with blank facades. Any future development on the neighbouring property will similarly provide a nil setback shared with the subject site and therefore no privacy impact will result.

To the west and south of the site, no visual privacy impacts will result due to the provision of blank facades and at-grade parking. Whilst the development includes a loading dock orientated to the west, the aural impacts have been considered within the Acoustic Report. To protect the aural privacy of future residents to the west, the proposal will include the provision of an acoustic wall and roller shutters. To the south, the Acoustic Report has also considered the vehicular movements in the at-grade parking area as it relates to the surrounding properties, in which it has found that there will be no adverse impacts. It is noted that on-site and street tree vegetation will be provided throughout the development which will also provide for a visual and acoustic buffer.

In light of the above, the development will not have any adverse impacts on visual privacy of neighbouring properties. Furthermore, the ongoing operation of the development will not adversely impact on acoustic privacy of existing and future neighbouring occupants.

### 5.3.5 Views

There are no protected or significant views through or over the site. Any views of the surrounding open farmland are only enjoyed given the site contains low density buildings and does not reflect the anticipated built form in accordance with the relevant development standards and controls. With this being said, the proposed development will be of a scale and form that is compatible with the desired future character of the locality and therefore, the proposal will have no adverse impact in regard to loss of views or outlook from adjoining properties nor public views in the locality.

### 5.3.6 Flora & Fauna Impacts

The subject site contains a variety of existing vegetation, the majority of which is identified as unimportant per the Arborist Report prepared by *Naturally Trees*. As is reasonably anticipated, the existing vegetation will be removed to accommodate the footprint and parking area of the proposed development. Whilst the existing vegetation is to be removed, the majority of trees (30) are identified as being of low and very low retention value, of which 19 are also identified as exempt species. Only one Category A tree is to be removed to accommodate the proposed development. In response to the loss of vegetation and to minimise any flora and fauna impacts, vegetation is to be replaced Landscape Plans prepared by *Ground Ink*. These landscaped plans demonstrate that new trees can be provided on site, alongside extensive low-level vegetation. This will increase vegetation in the locality and increase habitat opportunities. As such, the development is considered to have a positive impact on flora and fauna within the site and locality.

### 5.3.7 Micro-climate Impacts

The proposed development will have no significant adverse impact on surface or ground water regimes. Therefore, no significant impact on local micro-climate is anticipated.

As detailed above, the site is not subject to high levels of salinity in accordance with the laboratory testing provided as part of the Geotechnical Report prepared by *Geo-Logix*.

### 5.3.8 Water & Air Quality Impacts

During construction, appropriate sediment and erosion controls will be installed and maintained to prevent migration of sediment from the site, as per the accompanying Erosion and Sediment Control Plans prepared by *Henry & Hymas*.

Once constructed, roof and surface water from the development will be dispersed into the surrounding stormwater network (to be constructed as part of this application) and captured within on-site detention below natural ground. Stormwater plans prepared by *Henry & Hymas* accompany the application and provide details of stormwater management proposed for the site, including details of on-site detention. Therefore, the proposal will have no significant adverse impacts on the water quality of the locality.

In terms of air quality, the site will be managed during construction to mitigate any potential impacts on air quality, particularly during excavation. It is expected that appropriate dust mitigation measures will be employed during excavation. During operation, the proposed use is not anticipated to generate any unusual odour or fumes.

It is noted that an Odour Assessment has been prepared by *Todoroski Air Sciences* and is submitted with this application. The Odour Assessment has considered the impacts from surrounding developments, namely the nearby poultry farms. The assessment found that there will be no adverse impact from these nearby uses onto the subject site and is therefore considered acceptable.

As such, the proposal is unlikely to have any notable impact on air quality.

### 5.3.9 Aboriginal Heritage

An Aboriginal Cultural Heritage Assessment prepared by *Austral Archaeology* is submitted with this application. This Report has determined whether the proposal may harm any Aboriginal objects or places. The Report undertook an extensive Aboriginal Heritage Information Management Search, and whilst Aboriginal archaeological sites are registered within proximity, none are located on the subject site. These findings were further supported through desktop analysis and a visual inspection. Per Section 5 of the Report, recommendations are provided which should apply to the development during demolition, excavation and construction. As such, the proposal is considered acceptable with regards to Aboriginal Heritage.

## 5.4 ECONOMIC & SOCIAL IMPACTS

The proposal will have no adverse economic impacts. Undertaking the construction works will have some short-term positive economic impacts through employment generation, both direct employment and multiplier effects. Following construction, the provision of five retail tenancies, two kiosks, one anchor supermarket and two commercial tenancies will provide for uses serving the community, generating a significant number of jobs in the locality.

The proposal is considered to have strong positive social impacts. The provision of employment in a strategic location will improve the sites relationship to surrounding residential and educational land uses. Public domain enhancements will also add to the social fabric and vitality of the precinct, specifically, the proposed public piazza and north-south through-site link.

A Social Impact Assessment has also been prepared by *Planning Ingenuity* and is submitted with this application. Furthermore, crime prevention principles have been incorporated into the design, including well-designed lighting, maximising passive surveillance, clear pathways, and design to differentiate ownership between private and public space. These are provided in detail within the accompanying CPTED Report prepared by *Planning Ingenuity*.

## 5.5 THE SUITABILITY OF THE SITE

### 5.5.1 Access to Services

The subject site is located within an area earmarked for an increase in density and is strategically zoned as a B1 Neighbourhood Centre given its proximity to numerous residential land uses, education facilities and public open spaces. The site and proposal therefore enables excellent access to various land uses for the surrounding community.

An Essential Services Report is also submitted with this application which demonstrates that electricity, telephone, internet, gas, water and sewerage will be readily available to the subject site.

### 5.5.2 Car parking

The proposed development has been designed to provide adequate parking in accordance with requirements as stipulated within the DCP. **Table 5** below provides a summary of the use, requirement and proposed parking.

Table 5 Parking Provision			
Land Use	DCP	Requirement	Provision
Commercial Tenancies (Office or Business Premises)	1 space per 40m <sup>2</sup> GFA	1,858m <sup>2</sup> = 46.45 (46) spaces	Total provided: 303 spaces, including;

**Table 5 Parking Provision**

<i>Kiosks, Specialty Retail and Liquor Store (Retail premises less than 200m<sup>2</sup> rentable)</i>	1 space per 30m <sup>2</sup> GFA	1,721m <sup>2</sup> = 57.3 (57) spaces.  Note, this is predicated on future specialty retail tenancies being divided into smaller sizes, less than 200m <sup>2</sup> .	- 7 accessible - 6 direct to boot - 4 electric In addition, 16 on-street spaces are provided.
<i>Supermarket &amp; DTB (Retail Premises greater than 200m<sup>2</sup> rentable)</i>	1 space per 22m <sup>2</sup>	3,881m <sup>2</sup> = 176.4 (176) spaces	
	Total Required: 279 spaces		

In accordance with the above, the proposal is compliant with the DCP requirements. The proposed calculations are based on the rental area of each tenancy, noting that the specialty retail tenancies are capable of being subdivided into areas of less than 200m<sup>2</sup> via Complying Development Certificates (per the above calculations). Due to the excess parking, if specialty retail tenancies are maintained with an area greater than 200m<sup>2</sup>, the proposal will maintain compliance. It is noted that the DCP does not stipulate a minimum or maximum requirement and therefore, as the proposal meets the requirement, it is acceptable. The Traffic Impact Assessment prepared by *Colston Budd Rogers & Kafes* provides a detailed analysis of the parking for the uses and the subsequent provision of parking spaces.

In addition to the above, the proposal will provide 7 accessible parking spaces which will serve the needs of the future community. In terms of bicycle parking, the proposal provides a total of 24 spaces, with the capacity to increase this as the demand increases. Accordingly, the parking provision is compliant with the minimum requirements of the DCP for each proposed use.

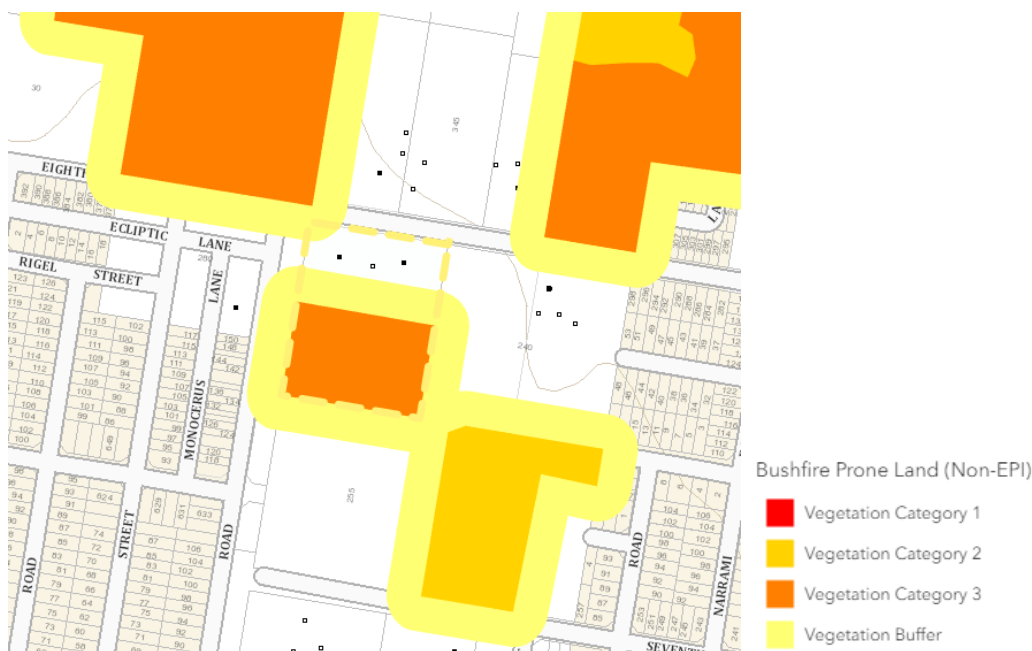
The proposal also provides a loading bay which will accommodate 19m heavy rigid vehicles. As detailed in the Traffic Report, the loading bay will operate with 14.4m heavy rigid vehicles in the interim, which will be increased to 19m, appropriately serve the loading, unloading and waste requirements of the development. The services and waste management will be in accordance with the DCP.

The Traffic Impact Assessment has also assessed vehicular movements and layout on-site and within the public domain, and has concluded this is compliant with the relevant Australian Standards. Furthermore, the traffic generation created by the proposal is also considered to be acceptable when compared to the existing and future development within the surrounding context and locality, particularly the anticipated increase of density per the SEPP.

### 5.5.3 Hazards

The subject site is not prone to landslip and is not flood affected land and the proposed development is not likely to increase the likelihood of such hazards occurring and is considered appropriate in this instance. However, it is noted that the subject site is affected by bushfire prone land and is considered below, per **Figure 22**.





**Figure 22** Bushfire Prone Land (Source: NSW Planning Portal)

Per the above, the subject site is affected by an area of Bushfire Prone Land, Category 3 and Vegetation Buffer. As such, a Bushfire Protection Assessment has been prepared by ABPP and is submitted with the application. The Bushfire Protection Assessment has found that the Category 3 Vegetation on the site has been removed, in addition to Category 2 Vegetation land to the south-east. The existing vegetation to the north-west is identified as Category 3, however, is also anticipated to be removed as it will accommodate a future RE1 Public Recreation zone.

Given the separation distances between the subject development and existing vegetation to the north-west, the Bushfire Protection Assessment has found that there will be no adverse hazard or risk from a bushfire occurrence on the neighbouring land. Accordingly, the proposed development has a low-level risk of bushfire and there are no triggers to apply any bushfire protection measures.

## 5.6 THE PUBLIC INTEREST

The proposed development will increase the supply and mixture of commercial floor space in the locality in a form that meets needs of the community and is consistent with the objectives of zone B1. The proposed development will provide high quality retail and commercial floor space that is compatible with the desired scale and character of development in the locality, and is suitable in terms of supporting the surrounding land uses. The inclusion of a variety of specialty tenancies, commercial premises, kiosks and an anchor supermarket, in a strategic location, is considered to be in the public interest.

Furthermore, the proposed development is consistent with the desired future character of the Eighth Avenue Neighbourhood Centre and Austral precinct, providing a development with a public piazza, north-south through-site link and extensive public domain improvements. Furthermore, the proposed development has also been designed to protect the amenity of future adjoining development.

As such, the proposed development is therefore considered to be in the public interest.



## 6. Conclusion

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This Statement accompanies a development application for the demolition of existing structures, excavation and construction of a commercial development involving the provision of an anchor supermarket, commercial and retail tenancies, public piazza and through-site link, with at-grade parking, associated landscaping and public domain works at No. 260 Eighth Avenue, Austral. The proposed development has been assessed pursuant to Section 4.15 of the Environmental Planning & Assessment Act, 1979 and the relevant planning Guidelines and Policies.

The proposed commercial development and public domain works are permissible with Council's consent in the zone B1 Neighbourhood Centre (and SP2 zone, where necessary) under the *State Environmental Planning Policy (Western Parkland City) 2021*. The proposed development complies with the applicable provisions of Western Parkland City SEPP and is also consistent with the objectives and controls of the *Liverpool Growth Centres Precinct Development Control Plan*, including *Schedule 1 Austral and Leppington North Precincts*, and the *Liverpool Development Control Plan 2008*.

The siting, design and external appearance of the proposed development is compatible with the desired future character of the locality and will not result in any unreasonable amenity impacts to future neighbouring properties. Furthermore, it will provide a variety of high-quality commercial uses in an accessible location to the surrounding residential, educational and public recreation zones. The proposal will also deliver public domain works which will improve the character of the site, both within the site boundaries and as part of a Works-In-Kind Agreement.

Conclusively, the site is suitable for the proposed development and satisfies the objectives of zone B1. The development will positively contribute to the quality of employment in the locality and will have manageable impacts on both the natural and built environment in the locality. There will be no adverse impacts on the natural environment the locality and the proposed development will have positive social and economic impacts for the area.

As the development is consistent with the desired future character of the Eighth Avenue Neighbourhood Centre, the proposal is considered to be in the public interest and worthy of the Liverpool City Council's support.

## ANNEXURE A

### State Environmental Planning Policy (Western Parkland City) 2021 – Compliance Table



## SEPP (WESTERN PARKLAND CITY) 2021 – APPENDIX 4 – COMPLIANCE TABLE

Clause / Control	Requirement	Proposal	Complies?
<b>Part 2 Permitted or prohibited development</b>			
2.3 Zone Objectives and Land Use Table	<p>Zone B1 Neighbourhood Centre</p> <p><b>3 Permitted with consent</b></p> <p>Amusement centres; Bed and breakfast accommodation; Boarding houses; Building identification signs; Business identification signs; <u>Business premises</u>; Centre-based child care facilities; Community facilities; Drainage; Educational establishments; Environmental facilities; Environmental protection works; Flood mitigation works; Food and drink premises; Home businesses; Home industries; Hostels; Hotel or motel accommodation; Kiosks; Medical centres; <u>Neighbourhood shops</u>; <u>Office premises</u>; Passenger transport facilities; Places of public worship; Public administration buildings; Recreation areas; <u>Roads</u>; Service stations; Serviced apartments; <u>Shops</u>; Shop top housing; Veterinary hospitals</p> <p><b>4 Prohibited</b></p> <p>Any development not specified in item 2 or 3</p>	<p><i>Shops; Business and Office Premises; Neighbourhood Shops; Roads</i> are all permitted with consent in Zone B1.</p> <p>It is noted that the proposed supermarket and specialised retail tenancies are identified as <i>shops</i>. The proposed commercial tenancies are identified as <i>business</i> and <i>office premises</i>.</p>	Yes
	<p><u>Zone Objectives</u></p> <p><i>To provide a range of small-scale retail, business and community uses that serve the needs of people who live or work in the surrounding neighbourhood.</i></p>	<p>The proposed development is consistent with the objective of the zone, providing a mixture of permissible uses, through a variety of compatible commercial, retail and supermarket tenancies in an accessible location in proximity to various zones. The development will provide short and long term employment, through the construction and operational phases. It will also promote sustainable transport, being located adjacent to numerous land uses and providing an improved public domain and accessibility through the site, to enhance the pedestrian environment. Further, the proposal will provide opportunities for much needed commercial uses within the B1 zone in the Austral precinct.</p>	Yes

## SEPP (WESTERN PARKLAND CITY) 2021 – APPENDIX 4 – COMPLIANCE TABLE

	<p>Zone SP2 Infrastructure</p> <p><b>2 Permitted without consent</b></p> <p>Roads</p> <p><b>3 Permitted with consent</b></p> <p>The purpose shown on the Land Zoning Map, including any development that is ordinarily incidental or ancillary to development for that purpose</p> <p><b>4 Prohibited</b></p> <p>Any development not specified in item 2 or 3</p>	The proposed roadworks located to the south-west of the subject site, which will form parts of the Works-In-Kind Agreement, are permitted within the SP2 zone.	Yes
	<p><u>Objectives of zone</u></p> <ul style="list-style-type: none"> <li>• To provide for infrastructure and related uses.</li> <li>• To prevent development that is not compatible with or that may detract from the provision of infrastructure.</li> </ul>	The proposal will provide roadworks and public domain works which will provide for infrastructure which will support the surrounding locality and community.	Yes
<b>Part 4 Principal development standards</b>			
4.3 Height of buildings	<p>(2) The height of a building on any land is not to exceed the maximum height shown for the land on the Height of Buildings Map.</p> <p><i>Maximum height: P1 - 17m</i></p>	The proposed development will provide a maximum height of 13.5m to the edge of the parapet fronting Eighth Avenue and is compliant with the development standard.	Yes
4.4 Floor Space Ratio	<p>(2) The floor space ratio for a building on any land is not to exceed the maximum floor space ratio shown for the land on the <a href="#">Floor Space Ratio Map</a>.</p> <p><i>No FSR applies to the subject site.</i></p>	Not applicable.	N/A
<b>Part 5 Miscellaneous provisions</b>			
5.1 Relevant Acquisition Authority	<p>(2) The authority of the State that will be the relevant authority to acquire land, if the land is required to be acquired under the owner-initiated acquisition provisions, is the authority of the State specified below in relation to the land shown on the Land Reservation Acquisition Map (or, if an authority of the State is not specified in relation to land required to</p>	No part of the site is identified to be acquired.	N/A



SEPP (WESTERN PARKLAND CITY) 2021 – APPENDIX 4 – COMPLIANCE TABLE			
	be so acquired, the authority designated or determined under those provisions).		
5.4 Controls relating to miscellaneous permissible uses	(7) Neighbourhood shops If development for the purposes of a neighbourhood shop is permitted under this Precinct Plan, the retail gross floor area must not exceed 100 square metres.	Shops are permitted with consent within the zone which differs from a neighbourhood shop. As such, the proposed supermarket will be identified as a shop and any retail tenancies will satisfy the shop definition per the SEPP.	Yes
<b>Part 6 Additional local provisions</b>			
6.1 Public utility infrastructure	(1) The consent authority must not grant development consent to development on land to which this Precinct Plan applies unless it is satisfied that any public utility infrastructure that is essential for the proposed development is available or that adequate arrangements have been made to make that infrastructure available when required.	Public utility infrastructure that is essential for the proposed development will be provided.	Yes

## ANNEXURE B

### Liverpool Growth Centres DCP – Compliance Table





LIVERPOOL GROWTH CENTRE PRECINCTS DCP– COMPLIANCE TABLE			
Clause / Control	Requirement	Proposal	Complies?
<b>Liverpool Growth Centre Precinct DCP – Main Body</b>			
<b>2.0 Precinct Planning Outcomes</b>			
2.2 The Indicative Layout Plan	1. All development applications are to be generally in accordance with the Indicative Layout Plan. 2. When assessing development applications, Council will consider the extent to which the proposed development is consistent with the Indicative Layout Plan. 3. Any proposed variations to the general arrangement of the Indicative Layout Plan must be demonstrated by the applicant, to Council's satisfaction, to be consistent with the Precinct Planning vision in the relevant Precinct Schedule.	The proposed development is generally in accordance with the ILP. This is discussed in Section 5.2.7 of this Statement. As above.	On merit  Yes
		The proposal seeks relatively minor variations from the ILP as it pertains to the north-south through-site link and is therefore acceptable in this regard.	Yes
2.3.1 Flooding	1. The pattern of subdivision is to ensure that no new dwelling will be located within the 1% Annual Exceedance Probability (AEP) flood extent shown on the <b>Flood Prone Land</b> figure in the relevant Precinct's Schedule.	The subject site is not affected by flooding and therefore the proposal is acceptable in this regard. It is noted that Stormwater Plans are submitted with this application.	Yes
2.3.2 Water cycle management	1. Management of 'minor' flows and 'major' flows within subdivisions and development sites is to be in accordance with Council's Engineering Specification. 2. Stormwater within new subdivisions is to be managed primarily through a gravity network of pipes and overland flows generally following streets where flow volumes exceed the capacity of pipes in accordance with Council's Engineering Specification. 3. All new development is to be connected, via the network described in control 1 above, to the Council's trunk drainage	This application is submitted with Stormwater Plans which deals with the flow and collection of water throughout the site.	Yes
		No subdivision is proposed.	N/A
		The proposed stormwater will connect to Council drainage systems.	Yes



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	<p>system shown on the Key elements of the water cycle management and ecology strategy figure, in the relevant Precinct Schedule.</p> <p>4. The acquisition of drainage easements over downstream properties, or inclusion of drainage easements on subdivision plans, will be required where direct access to Council's drainage system.</p> <p>5. Roads on primary drainage lines shown on the Key elements of the water cycle management and ecology strategy figure, in the relevant Precinct Schedule, are to be constructed in the locations shown (subject to detailed survey and subdivision design), and are to be designed in accordance with specifications of Council in relation to management of stormwater flows and quality.</p> <p>6. The developed 1%, 20% and 50% AEP peak flows are to be maintained at pre-development flows through the incorporation of stormwater detention and management devices. Where subdivision works occur prior to the completion of required trunk drainage works, temporary on site facilities need to be provided in order to limit drainage volume and velocity to that experienced prior to development.</p> <p>7. Where development includes the construction of water quality treatment infrastructure, the infrastructure is to be constructed in accordance with the Precinct Water Cycle Management Strategy (available from Council) and Council's Engineering Specification. The applicant must demonstrate that the proposed infrastructure will achieve the water quality targets in Table 2-1.</p> <p>8. Trunk drainage channels are to be designed and constructed as naturalised channels where possible.</p>	<p>Noted.</p> <p>Refer to submitted civil plans which include the location of roadways and stormwater.</p> <p>Refer to supporting documentation.</p> <p>Refer to supporting documentation.</p> <p>Noted.</p>	<p>-</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>-</p>
2.3.3 Salinity and soil management	<p>1. Development applications, that include earthworks, on land with a low, or moderate to high risk of salinity (identified in the <b>Areas of potential salinity risk map</b>), are to be accompanied by</p>	<p>The subject site is identified within an area of Low to Medium Salinity. A Geotechnical Report is submitted with this application which addresses ground conditions, including salinity. To ensure that works are in</p>	<p>Yes</p>

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	<p>information detailing how the design and construction of the proposed subdivision intends to address salinity issues. All works are to comply with the Western Sydney Salinity Code of Practice 2004 (WSROC) and <b>Appendix B</b>.</p> <p>2. Salinity and sodicity management related to <b>Appendix B</b> is to complement WSUD strategies, improving or at least maintaining the current condition, without detriment to the waterway environment.</p> <p>3. All development must incorporate soil conservation measures to minimise soil erosion and siltation during construction and following completion of development. Soil and Water Management Plans, prepared in accordance with Managing Urban Stormwater - Soils and Construction (Landcom 3rd Edition March 2004 ('The Blue Book')) are to be submitted with each relevant subdivision Development Application.</p> <p>4. Salinity shall be considered during the planning, design and carrying out of earthworks, rehabilitation works and during the siting, design and construction of all development including infrastructure:</p> <ul style="list-style-type: none"> <li>• To protect development and other works from salinity damage; and</li> <li>• To minimise the potential impacts that development and other works may have on salinity.</li> </ul>	<p>accordance with the salinity practice code, a condition of consent can be imposed.</p> <p>Can be imposed as a condition of consent.</p> <p>An Erosion and Sediment Control Plan is submitted with this application.</p> <p>As above. It is noted that excavation is generally minimise and will therefore have no adverse impact to salinity.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p>
2.3.4 Aboriginal and European heritage	<p>1. Development applications must identify any areas of Aboriginal heritage value that are within or adjoining the area of the proposed development, including any areas within the development site that are to be retained and protected (and identify the management protocols for these).</p> <p>2. Developments or other activities that will impact on Aboriginal heritage may require consent from the Office of Environment and</p>	<p>An Aboriginal Cultural Heritage Due Diligence Assessment prepared by <i>Austral Archaeology</i> is submitted with this application which deems that the proposal will not have any impact to Aboriginal heritage. It is noted that the site is not subject to Aboriginal Cultural Heritage per the mapping.</p> <p>As above.</p>	<p>Yes</p> <p>Yes</p>



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<p>Heritage (OEH) under the <i>National Parks and Wildlife Act 1974</i> and consultation with the relevant Aboriginal communities.</p> <p>3. Any development application that is within or adjacent to land that contains a known Aboriginal cultural heritage site, as indicated on the <b>Aboriginal cultural heritage sites</b> figure, in the relevant Precinct Schedule, must consider and comply with the requirements of the <i>National Parks and Wildlife Act, 1974</i>.</p> <p>5. Applications for subdivision and building on the properties identified on the European cultural heritage sites figure, in the relevant Precinct's Schedule.</p> <p>6. Features which contribute to the heritage significance of the item or conservation area are to be conserved.</p> <p>7. Features which contribute to an understanding of the history of the item, or key periods of its development, are to be conserved.</p> <p>8. Significant landscape elements and/or views associated with the item are to be conserved.</p> <p>9. Significant historical property boundaries, if identified as part of the significance of the item, are to be conserved.</p> <p>10. Significant uses, if identified as part of the significance of the item, are to be conserved or a similar/compatible use identified for the heritage item where possible.</p> <p>11. Unsympathetic elements are to be removed from the item or conservation area, where this will contribute to the heritage significance of the item or conservation area.</p> <p>12. New work in the vicinity of built heritage items should be readily identifiable as such, and be sympathetic to the form, scale, massing, setback and overall character of the item, and should not detract from its appreciation.</p> <p>13. Alterations and additions are to be located away from significant and/or primary elevations, and behind and below the main ridge line of built heritage items.</p>	<p>No Aboriginal cultural heritage sites are near the subject site.</p>	<p>Yes</p>
	<p>No European Heritage is located within proximity to the site per the figure in the Precinct Schedule DCP.</p>	<p>Yes</p>
	<p>No heritage significant elements are on or within proximity to the site.</p>	<p>Yes</p>
	<p>As above, there will be no impact.</p>	<p>Yes</p>
	<p>There are no significant landscaped elements on the site and therefore there will be no impact.</p>	<p>Yes</p>
	<p>No significant historical property boundaries are present on the subject site.</p>	<p>Yes</p>
	<p>The site is currently used as a residential dwelling and contains no significant use.</p>	<p>Yes</p>
	<p>Not applicable.</p>	<p>N/A</p>
	<p>No heritage items in vicinity of the subject site.</p>	<p>N/A</p>
	<p>Not applicable.</p>	<p>N/A</p>

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		14. Existing fabric, use, associations and meanings are to be adequately recorded before any changes are made	Not applicable.	N/A
2.3.5 Native vegetation and ecology	Native and	1. Native trees and other vegetation are to be retained where possible by careful planning of development (particularly at the subdivision stage) to incorporate trees into areas such as road reserves and private or communal open space.	The subject site does not contain any significant vegetation. The vegetation to be removed from the site will be replaced according to the submitted Landscape Plan and is considered to be entirely reasonable as is anticipated by the B1 zone.	Yes
		2. When assessing an application that proposes removal of a tree or trees, Council will consider whether the tree or trees.	Vegetation to be removed is reasonable to accommodate a building reflective of the B1 zone. This will be replaced accordingly.	Yes
		3. All existing indigenous trees shall be retained or replaced where removal is unavoidable. Where approval is given to remove trees, appropriate replacement planting using similar species will be required.	Removal of trees will be replaced according to the submitted landscaping plan, which includes indigenous species.	Yes
		4. The design of a development should demonstrate that existing street trees will be retained to the greatest practical extent.	Not applicable, no existing street trees.	N/A
		5. Buildings are to be set back a minimum of 3 metres from existing trees that are to be retained.	Not applicable as all vegetation is to be removed as is reasonably anticipated.	N/A
		6. The design and location of access driveways should wherever possible be located to avoid or minimise removal of existing street trees.	Driveway location responds to the new half roads to be built. Street tree vegetation will be provided.	Yes
		7. Council may consider alternative street cross section designs where the typical cross section would result in removal of existing street trees that could otherwise be retained.	The proposed street design will include appropriate street trees.	Yes
		8. Where practical, prior to development commencing, applicants are to: • provide for the appropriate re-use of native plants and topsoil that contains known or potential native seed bank; and • relocate native animals from development sites. Applicants must refer to OEH's <i>Policy on the Translocation of Threatened Fauna in NSW</i> .	As detailed, all existing vegetation is to be removed and replaced at a high standard.	Yes
		9. Within land that is in the <b>Environmental Protection Overlay</b> , as shown on the Indicative Layout Plan in the relevant Precinct	Not applicable. Site is not identified within Environmental Protection Overlay.	N/A

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	<p>Schedule, all native vegetation is to be retained and rehabilitated, except where clearing is required for essential infrastructure such as roads and drainage and where that clearing is consistent with the Growth Centres Biodiversity Certification and the Growth Centres Strategic Assessment Program<sup>1</sup>; and</p> <p>10. Within land that is in a <b>Riparian Protection Area</b> (refer to the Riparian Protection Areas Figure in the relevant Precinct Schedule) native vegetation is to be conserved and managed in accordance with the Guidelines for riparian corridors on waterfront land prepared by the NSW Office of Water (available at <a href="http://www.water.nsw.gov.au">www.water.nsw.gov.au</a>).</p> <p>11. Development on land that adjoins land zoned E2 Environmental Conservation is to ensure that there are no significant detrimental impacts to the native vegetation and ecological values of the E2 zone.</p> <p>12. All subdivision design and bulk earthworks are to consider the need to minimise weed dispersion and to eradicate weeds on site. If Council believes that a significant weed risk exists, a Weed Eradication and Management Plan outlining weed control measures during and after construction is to be submitted with the subdivision DA.</p> <p>13. A landscape plan is to be submitted with all subdivision development applications, identifying:</p> <ul style="list-style-type: none"> <li>• all existing trees on the development site and those that are proposed to be removed or retained;</li> <li>• the proposed means of protecting trees to be retained during both construction of subdivision works and construction of buildings;</li> <li>• proposed landscaping including the locations and species of trees, shrubs and ground cover to be planted as part of subdivision works;</li> </ul>	<p>Not applicable. Site is not within a Riparian Protection Area.</p>     <p>Not applicable. Site does not adjoin E2 land.</p>     <p>This can be imposed as a condition of consent.</p>     <p>Landscape Plans prepared by Ground Ink are submitted with this application. This has considered: Existing vegetation to be removed and replaced as is anticipated.</p> <p>As above.</p>  <p>Landscaping provided throughout the site as is appropriate for the B1 zone.</p>	<p>N/A</p>     <p>N/A</p>     <p>Yes</p>     <p>Yes</p> <p>Yes</p>  <p>Yes</p>

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	<ul style="list-style-type: none"> <li>the relationship of the proposed landscaping to native vegetation that is to be retained within public land, including factors such as the potential for weed or exotic species invasion and the contribution of the proposed landscaping to the creation of habitat values and ecological linkages throughout the Precinct; and</li> <li>How bushfire risk has been managed, including requirements for Asset Protection Zones and how these relate to the proposed landscaping.</li> </ul> <p>14. The selection of trees and other landscaping plants is to consider:</p> <ul style="list-style-type: none"> <li>The prescribed trees in <b>Appendix C</b>;</li> <li>The use of locally indigenous species where available;</li> <li>Bushfire risk;</li> <li>Contribution to the management of soil salinity, groundwater levels and soil erosion;</li> <li>Items of environmental heritage, heritage conservation areas, historic road alignments and significant view lines.</li> </ul> <p>15. For the purposes of clause 5.9 of the relevant Precinct Plan, prescribed trees include:</p> <ul style="list-style-type: none"> <li>Trees taller than the minimum height and greater than the minimum trunk diameter specified in <b>Appendix C</b>, and</li> <li>Tree species listed in <b>Appendix C</b>.</li> </ul>	<p>Native vegetation is to be provided on the site and within the future public domain per the Landscape Plan.</p> <p>Landscaping has considered bushfire. A Bushfire Report has also been submitted with this application.</p> <p>Refer to Landscape Plan.</p> <p>As above.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>
2.3.6 Bushfire hazard management	<p>1. Reference is to be made to Planning for Bushfire Protection 2006 in subdivision planning and design and development is to be consistent with Planning for Bushfire Protection 2006.</p> <p>2. Subject to detailed design at development application stage, the indicative location and widths of Asset Protection Zones (APZs) are to be provided generally in accordance with the <b>Bushfire risk and Asset Protection Zone Requirements</b> figure in the relevant Precinct Schedule. APZs and construction standards are to be</p>	<p>A Bushfire Assessment Report is submitted with this application.</p> <p>As above.</p>	<p>Yes</p> <p>Yes</p>

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	<p>accurately mapped and detailed for each affected lot on plans submitted with the development application.</p> <p>3. APZs:</p> <ul style="list-style-type: none"> <li>• are to be located wholly within the Precinct;</li> <li>• may incorporate roads and flood prone land,</li> <li>• are preferred to be located wholly outside of a riparian zone.</li> </ul> <p>APZs may only be permitted within a riparian zone where compliant with the NSW Office of Water requirements,</p> <ul style="list-style-type: none"> <li>• may be used for open space and recreation subject to appropriate fuel management,</li> <li>• are to be maintained in accordance with the guidelines in Planning for Bushfire Protection 2006,</li> <li>• may incorporate private residential land, but only within the building setback (no dwellings are to be located within the APZ),</li> <li>• are not to increase the maintenance burden on public lands, and</li> <li>• are to be generally bounded by or incorporate a public road or perimeter fire trail that is linked to the public road system at regular intervals in accordance with <i>Planning for Bushfire Protection 2006</i>. <p>4. Establishment and maintenance of the APZ must not require clearing of native vegetation within any Native Vegetation Protection Areas or Existing Native Vegetation Areas shown on the Native Vegetation Protection Map.</p> <p>5. Vegetation outside Riparian Protection Areas, Native Vegetation Protection Areas and Existing Native Vegetation Areas is to be designed and managed as a ‘fuel reduced area’ where it forms part of an APZ.</p> <p>6. Where an allotment fronts and partially incorporates an APZ it shall have an appropriate depth to accommodate a dwelling with private open space and the minimum required APZ. The APZ will be identified through a Section 88B instrument.</p> <p>7. Temporary APZs, identified through a Section 88B instrument, will be required where development is proposed on allotments</p> </li></ul>	<p>Refer to Bushfire Report and refer to Section 5.5.3 of this Statement.</p>          <p>Where vegetation is proposed to be removed, this is considered in the Bushfire Report. It is noted that redevelopment of the locality will further remove bushfire impacts.</p> <p>Noted.</p>   <p>Refer to Bushfire Assessment Report.</p>   <p>Noted.</p>	<p>Yes</p>          <p>Yes</p> <p>-</p>   <p>Yes</p>   <p>-</p>



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	<p>next to undeveloped land that presents a bushfire hazard. Once the adjacent stage of development is undertaken, the temporary APZ will no longer be required and shall cease.</p> <p>8. Reticulated water is to meet the standards contained within Planning for Bushfire Protection 2006. Water supply is to be via a ring main system, engineered to the requirements of Australian Standard 2419.1-1994 Fire Hydrant Installations.</p> <p>9. Buildings adjacent to APZs are to be constructed in accordance with the requirements of Appendix 3 of Planning for Bushfire Protection 2006 and Australian Standard 3959-1999 - Construction of Building in Bushfire Prone Areas.</p>	<p>Noted.</p> <p>Refer to Bushfire Assessment Report.</p>	<p>-</p> <p>Yes</p>
2.3.7 Site contamination	<p>1. All subdivision Development Applications, and applications proposing a change of use to a more sensitive land use (e.g. Residential, education, public recreation facility etc), shall be accompanied by a Stage 1 Preliminary Site Investigation prepared in accordance with the NSW EPA Contaminated Sites Guidelines, State Environmental Planning Policy 55 – Remediation of Land and the <i>Contaminated Land Management Act, 1995</i> and relevant Council Policies.</p> <p>2. Where the Stage 1 Investigation identifies potential or actual site contamination a Stage 2 Detailed Site Investigation must be prepared in accordance with the NSW EPA Contaminated Sites Guidelines, State Environmental Planning Policy 55 – Remediation of Land and the Contaminated Land Management Act, 1995 and any relevant Council Policies. A Remediation Action Plan (RAP) will be required to be submitted and approved by Council prior to development consent being granted for areas identified as contaminated land in the Stage 2 Site Investigation.</p> <p>3. DAs for development in “high risk” areas of <b>potential contamination risk-ranking</b> figure shall be accompanied by a Stage 2 Detailed Environmental Site Investigation.</p>	<p>A Detail Site Investigation prepared by <i>Geo-Logix</i> is submitted with this application with deals appropriately with contamination.</p> <p>Refer to DSI.</p> <p>The subject site is not identified as within an area of high risk contamination per the DCP.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p>

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	<p>4. All investigation, reporting and identified remediation works must be in accordance with the NSW EPA's (now Office of Environment and Heritage) Guidelines for Consultants Reporting on Contaminated Sites and SEPP 55 – Contaminated Land and relevant Council Policies.</p> <p>5. Prior to granting development consent, the Consent Authority must be satisfied that the site is suitable, or can be made suitable, for the proposed use. Remediation works identified in any RAP will require development consent prior to the works commencing.</p> <p>6. Council may require a Site Audit Statement (SAS) (issued by an NSW Accredited Site Auditor) to be provided at any stage of the contamination investigation, remediation or validation stages.</p>	<p>As above. RAP will be subject to a condition of consent.</p> <p>As above.</p> <p>Noted.</p>	<p>Yes</p> <p>Yes</p> <p>-</p>
2.3.8 Development on and adjacent to electricity and gas easements	<p>1. Subdivision of land that is affected by easements and land adjacent to easements, as shown on the <b>Location of Easements</b> figure in the relevant Precinct Schedule, is to be consistent with the controls in this part of the DCP, and any specific controls in the Precinct Schedule.</p>	<p>The site is adjacent to the existing Electricity Easement. This has been considered as part of the subject application and no part of the proposed building is located over the easement.</p>	<p>Yes</p>
2.3.9 Noise	<p>1. Figure 2-2 provides guidance to applicants on measures to mitigate the impacts of rail and traffic noise within the Precinct.</p> <p>2. Development Applications must be accompanied by an acoustic report where the development is in a location, shown on the <b>Potential noise attenuation measures</b> figure in the relevant Precinct Schedule, such as:</p> <ul style="list-style-type: none"> <li>3. adjacent to a railway line, arterial road, sub-arterial road, transit boulevard or other road with traffic volumes predicted to exceed (or currently exceeding) 6,000 vehicles per day;</li> <li>4. potentially impacted upon by a nearby industrial / employment area; or</li> <li>5. potentially impacting upon sensitive receivers such as residences within the precinct and outside the precinct.</li> </ul>	<p>Not applicable.</p> <p>An Acoustic Report prepared by <i>Acoustic Logic</i> is submitted with this application which addresses the potential impact created by the proposal onto the neighbouring properties. As the proposal does not involve the provision of any residential accommodation, there will be no impact to future users from the surrounding locality. The acoustic component is discussed in Section 5.3.8 of this Statement.</p>	<p>N/A</p> <p>Yes</p>

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	<p>6. The acoustic report shall demonstrate that the noise criteria in Development Near Rail Corridors and Busy Roads- Interim Guideline (Department of Planning 2008) have been considered.</p> <p>7. Subdivision design on land adjacent to significant noise sources is to consider and implement measures to attenuate noise within dwellings and in external areas that are classified as Principle Private Open Space (refer to <b>clause 4.2.7</b>)</p> <p>8. Physical noise barriers (ie. Noise walls or solid fencing) are not generally supported, and measures to attenuate noise through subdivision layout, such as setbacks, building orientation, and building design and materials selection should be implemented to achieve appropriate internal noise standards.</p>	<p>Not applicable as site does not adjoin a rail corridor or busy road.</p> <p>Not applicable.</p> <p>Refer to Acoustic Report.</p>	<p>-</p> <p>N/A</p> <p>-</p>
2.3.10 Odour assessment and control	Odour management is subject to the Protection of the Environment Operations Act 1997. Currently the only methods of controlling odour impacts are applying buffers around odour generating activities and industry best management practices.	This application is submitted with an Odour Assessment prepared by <i>Todoroski Air Sciences</i> . This concludes that the proposal is considered acceptable.	Yes
2.3.11 Air quality	<p>1. For industrial / employment developments, the emission of all air impurities is to be strictly controlled in accordance with the Protection of the Environment Operations (Clean Air) Regulation 2002 and must not exceed the prescribed standard concentration and emission rates.</p> <p>2. Implement effective site controls during and after demolition and construction to ensure that development does not contribute to increased air pollution.</p>	<p>Not applicable.</p> <p>Conditions of consent can be imposed.</p>	<p>N/A</p> <p>Yes</p>
2.4 Demolition	<p>1. All demolition work must comply with the Australian Standard AS2601 - 1991, The Demolition of Structures.</p> <p>2. Security fencing such as hoardings must be provided around the perimeter of the demolition site prior to work commencing to prevent access by unauthorised persons at all times during the</p>	<p>Demolition works to be in accordance with the Australian Standards.</p> <p>Noted.</p>	<p>Yes</p> <p>-</p>

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	<p>demolition period. Approval of the fencing by Council must be received prior to erection.</p> <p>3. All lead contaminated materials identified in the building must be handled and disposed of in accordance with the NSW Environment Protection Authority's requirements.</p> <p>4. Dust controls must be implemented on site prior to and during demolition.</p> <p>5. Hazardous materials audits shall be conducted on any buildings at the site that may require demolition.</p> <p>6. Asbestos, if identified in the building, must be removed and disposed of in accordance with the requirements of Work Cover</p> <p>9. A Waste Management Plan (WMP) is to be submitted with the Development Application. The WMP must include volume or area estimates and information about reuse, recycling and disposal options for all types of waste produced on-site, including excavation materials.</p> <p>11. A Dilapidation Report may be required to be submitted with a Development Application for any demolition within the zone of influence of any other building.</p>	<p>Noted, this will be removed per the relevant standards if required.</p> <p>Refer to Erosion and Sediment Control Plan.</p> <p>This can be imposed as a condition of consent.</p> <p>Noted, this will be undertaken in accordance with standard practice.</p> <p>A Waste Management Plan prepared by <i>Elephants Foot</i> is submitted with this application.</p> <p>This can be imposed as a condition of consent if required.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>
2.5 Crime Prevention through Environmental Design	<p>1. Buildings should be designed to overlook streets, lanes and other public or communal areas to provide casual surveillance. In the case of corner lots habitable windows are also be oriented to overlook the side street.</p> <p>2. The design of all development is to enhance public surveillance of public streets and open space/conservation areas.</p> <p>3. For residential development, the use of roller shutters other than garages is not permitted on doors and windows facing the street. Any security railings must be designed to complement the architecture of the building.</p> <p>4. Developments are to avoid creating areas for concealment and blank walls facing the street.</p>	<p>A CPTED Report prepared by <i>Planning Ingenuity</i> is submitted with this application. Casual surveillance is maximised by the proposed development through orientation of uses.</p> <p>As above.</p> <p>Not proposed.</p> <p>No areas for concealment are proposed as clear sightlines are provided. In addition, blank walls include the provision of vertical planting as to minimise the opportunity for graffiti.</p>	<p>Yes</p> <p>Yes</p> <p>N/A</p> <p>Yes</p>

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	<p>5. Pedestrian and communal areas are to have sufficient lighting to ensure a high level of safety. These areas must be designed to minimise opportunities for concealment.</p> <p>6. All developments are to incorporate the principles of Crime Prevention Through Environmental Design (CPTED). Development Applications for subdivision, public open space, community facilities, commercial developments, mixed-use developments, and schools may require a formal crime risk (CPTED) assessment as part of the EP&amp;A Act 1979.</p>	<p>Lighting will be provided to all communal spaces particularly the public pizza and north-south through-site link.</p> <p>Refer to CPTED Report.</p>	<p>Yes</p> <p>Yes</p>
2.6 Earthworks	<p>1. Subdivision and building work is to be designed to respond to the natural topography of the site wherever possible, minimising the extent of cut and fill both during subdivision and when buildings are constructed.</p> <p>2. The applicant is to demonstrate how the finished land levels will be integrated with nearby land and facilitate appropriate drainage.</p> <p>3. Where terraced retaining walls are proposed the minimum horizontal distance between each step is one metre.</p> <p>4. A variation to the retaining wall heights can be considered with supporting justification.</p> <p>5. Council will consider permitting greater cut for basements.</p> <p>6. All retaining walls proposed are to be identified in the development application. Those affecting adjoining properties i.e. adjacent to property boundaries are to be available for inspection prior to the internal linings of the house being installed. All other approved retaining walls are to be in place prior to the issue of an occupation certificate.</p> <p>7. Where cut or fill is proposed on the boundary of a lot, retaining walls are to be constructed with side fence posts integrated with the retaining wall (relevant construction details are required with retaining wall approval).</p>	<p>The proposal will require a degree of excavation to accommodate the proposed built form and is considered to be reasonably anticipated for a development in the B1 zone. Excavation has been minimised and no basement parking is proposed.</p> <p>The proposal will maintain at-grade access around the periphery of the built form and will facilitate appropriate drainage.</p> <p>No significant terracing is proposed. Refer to architectural and landscape plans.</p> <p>The proposed retaining wall heights are acceptable predicated on the topography of the site and are generally minimised.</p> <p>No basement is proposed.</p> <p>Noted, refer to architectural plans.</p> <p>Refer to supporting documentation submitted with this application.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>-</p> <p>Yes</p>





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	8. Retaining walls that front a public place are to be finished with anti-graffiti coating.	Noted. This can be imposed as a condition of consent.	-
	9. Retaining walls are to be designed and constructed to allow for installation of boundary fencing without impact on the structural soundness of the retaining wall and its footings.	Noted.	-
	10. A Validation Report is required to be submitted to Council prior to the placement of imported fill on site. All fill shall comply with the NSW Office of Water – “ <i>Site Investigation for Urban Salinity</i> ” and the OEH Contaminated Sites Guidelines – “ <i>Guidelines for the NSW Site Auditor Scheme (2nd edition) – Soil Investigation Levels for Urban Development Sites in NSW</i> ”.	Noted.	-
	11. Earth moved from areas containing noxious weed material must be disposed of at an approved waste management facility, and transported in compliance with the <i>Noxious Weeds Act 1993</i> .	Noted.	-
	12. Development on land having a natural gradient of 1:6.7 (15%) or greater shall be accompanied by a geotechnical study, including guidelines for structural and engineering works on the land.	A Geotechnical Report prepared by <i>Geo-Logix</i> is submitted with application.	Yes
	13. For sites with existing water storage facilities (dams) the DA must include a dam removal plan which addresses each of the following controls to Council’s satisfaction and must also include details of: • A water quality and soil test which details any contaminants in both the water and soil at the base of the dam (all testing shall be undertaken by a qualified consultant and National Association of Testing Authorities accredited laboratory). • A salinity hazard test undertaken in accordance with the Office of Water salinity site assessment guidelines.	No existing dams are located on the subject site.	Yes
	14. Sites identified as contaminated must follow the Office of Environment and Heritage contaminated water or soil removal guidelines in the <i>National Environment Protection (Assessment of Site Contamination) Measure 1999</i> . Contaminated water should be disposed of at a liquid waste facility.	Refer to Preliminary Site Investigation.	Yes



LIVERPOOL GROWTH CENTRE PRECINCTS DCP– COMPLIANCE TABLE			
	15. Water identified as not contaminated may be re-used on site or on other properties. Should there be no possible reuse option for the water; a controlled release into the creek may be possible.	Noted.	-
	16. Any controlled release of water into the receiving waters (creek) must ensure against any erosion impact.	Noted.	-
	17. It is recommended that any water release is undertaken during high flow events as creek water quality is reduced at this time.	Noted.	-
<b>3.0 Neighbourhood and Subdivision Design</b>			
3.1.1 Residential Density	1. All applications for residential subdivision and the construction of residential buildings are to demonstrate that the proposal meets the minimum residential density requirements of the relevant Precinct Plan and contributes to meeting the overall dwelling target in the relevant Precinct.	Not applicable.	N/A
3.3.1 Street network layout and design	<p>1. The design and construction of streets is to be consistent with the relevant typical designs in Figure 3-11 to Figure 3-18, Council's Engineering Specifications and Austroads.</p> <p>2. The typical designs in Figure 3-11 to Figure 3-18 are based on minimum dimensions and the design of streets may need to be modified to incorporate water sensitive urban design measures and to ensure appropriate site drainage.</p> <p>3. All Collector Roads, Sub-arterial Roads, Arterial Roads and Transit Boulevards, and local streets which form part of a bus route identified by the Transport for NSW, are to have at least one travel lane in each direction with a minimum width of 3.5 metres, suitable for buses. Lanes which are not adjacent to a kerb may be 3.2m wide. Intersections on bus routes are to be designed to accommodate bus manoeuvrability.</p> <p>4. Alternative street designs for local streets and access ways may be permitted on a case by case basis if they preserve the functional objectives and requirements of the design standards.</p>	<p>The proposed roadways will be constructed according to Council's specifications. Refer to Civil Documentation prepared by <i>Henry &amp; Hymas</i> which provides details of the proposed roadways.</p> <p>Refer to Civil Documentation submitted with this application.</p> <p>Noted.</p> <p>Refer to Civil Documentation.</p>	<p>Yes</p> <p>Yes</p> <p>-</p> <p>Yes</p>



## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

5. Roads in the relevant Precinct are to be constructed in accordance with the hierarchy shown on the Precinct road hierarchy figure in the relevant Precinct Schedule.
6. The locations and alignments of all roads are to be generally in accordance with the locations shown on the Precinct road hierarchy figure in the relevant Precinct Schedule.
7. Where any variation to the residential street network indicated at the Precinct road hierarchy figure, is proposed, the alternative street network is to be designed

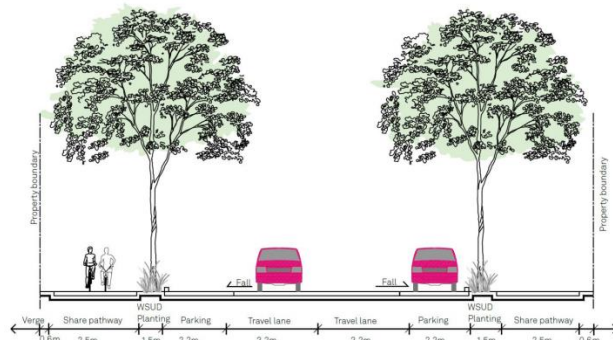


Figure 3-13: Typical collector road

Noted, the proposal is consistent with the ILP in terms of roadway design. This is shown in the Civil Documentation and discussed in this Statement.

Yes

As above, the proposal aligns with the ILP road layout and is considered acceptable.

Yes

The proposed road layouts are considered suitable in accordance with the requirements of the DCP. This is discussed under Section 4.10 of this Statement.

Yes

## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

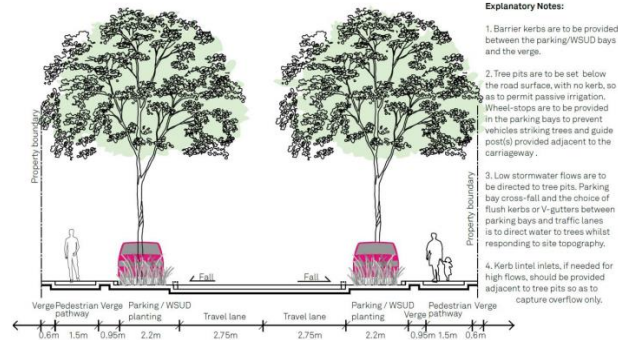


Figure 3-15: Typical local street

8. Variations to the residential street network as permitted under control 7 above will only be approved by Council where the applicant can demonstrate to Council's satisfaction that the proposal:

- will not detrimentally impact on access to adjoining properties,
- provides for the management of stormwater to drain to Council's trunk drainage network, without negative impacts on other properties,
- will not impede the orderly development of adjoining properties in accordance with the relevant Precinct Plan and this Development Control Plan, and
- does not restrict the ability to provide water, sewer, electricity and other essential services to the development or to development on adjoining properties.

11. Where roads are adjacent to public open space or drainage land, or adjacent to arterial, sub-arterial or transit boulevards, the verge width on the side adjacent to the open space, drainage land or major road may, in certain circumstances, be reduced to a minimum of 1m

The proposed development is in accordance with the road network requirements of the DCP. This is discussed in Section 4.10 of this Statement.

Yes

Not applicable, the roadway is not adjacent to public open space, drainage land or arterial roads.

N/A

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	<p>12. Where local roads are located as per control 11 above or are within or on the boundary of land zoned Environmental Living, the parking bays, or parking lanes may be provided on one side only providing the applicant can demonstrate to Council's satisfaction that the road will operate safely and effectively. Street trees are still to be provided.</p> <p>13. Where streets are proposed as part of an application for subdivision that are located adjacent to public recreation land, drainage land, community facilities or schools, the applicant will be responsible for construction of the full width of the street, unless Council specifies otherwise.</p> <p>14. Except where otherwise provided for in this DCP, all streets and roundabouts are to be designed and constructed in accordance with the minimum requirements set out in Council's Engineering Specifications.</p> <p>15. Local streets which are located within an existing road reserve are to be designed and constructed in accordance with Figure 3-16</p> <p>16. On sloping land, roads that cross the slope may incorporate split carriageways so as to minimise cut and fill, and provide opportunities for landscaping and the preservation of trees.</p> <p>17. Private roads are to be designed and constructed in accordance with Council's Engineering Specifications</p>	<p>The proposed development will provide a local road to the road identified as Auger Street. This will provide parking bays on Eighth Avenue and will operate safely per the submitted documentation.</p> <p>The applicant will only upgrade half of the road to Eighth Avenue and is considered acceptable as this is entirely consistent with the road construction for the remainder of Eighth Avenue.</p> <p>Noted.</p> <p>Noted, refer to Civil Documentation.</p> <p>Noted, refer to Civil Documentation.</p> <p>Not applicable, no private roads are proposed.</p>	<p>Yes</p> <p>Yes</p> <p>-</p> <p>-</p> <p>-</p> <p>N/A</p>
3.3.2 Street Furniture	<p>1. Street trees are required for all streets.</p> <p>2. Passively irrigated street trees should be provided for any streets in which trees are located in tree pits (such as local streets as per Figure 3-15).</p> <p>3. The spacing of street trees will relate to the subdivision lot widths, and street type, and shall be provided in accordance with Table 3-5.</p>	<p>Street trees will be provided for all streets.</p> <p>Noted.</p> <p>Noted.</p>	<p>Yes</p> <p>-</p> <p>-</p>



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3.3.3 Local Area Traffic Management	1. A Local Area Traffic Management (LATM) plan shall be submitted with any development which involves the opening of a new road(s), or modifications to existing roads. Design solutions shall conform to Austroads Guide to Traffic Management Part 8 (Local Area Traffic Management).	The proposal will involve extension to existing roadways which will not create any traffic conflict. This can be imposed as a condition of consent if required.	Yes
	2. New local streets and collector roads should be designed to encourage a low speed environment.	Noted.	-
	3. Council's preferred traffic calming devices are landscaped kerb extensions with a visually distinctive road surface	Noted.	-
	4. Intersections between busier local streets and collector roads, collector roads with other higher order roads, or intersection legs with a stop sign shall generally be fitted with pedestrian refuges, to facilitate non-vehicular crossing and to provide a visual reinforcement of the intersection.	Noted, this can be imposed as a condition of consent if required.	-
	5. All other intersections between intersecting local streets, and local streets with collector roads, shall have a textured surface treatment.	Noted, as above, this can be imposed as a condition of consent.	-
	6. To reinforce the road hierarchy and to reduce the ability for vehicles to attain high speeds, local streets should not be given priority for a distance of greater than 400m.	Noted.	-
3.3.6 Pedestrian and Cycle Network	1. Key pedestrian and cycleway routes are to be provided generally in accordance with the pedestrian and cycleway network figure in the Precinct Schedule.	Noted, pedestrian and cycle access will be provided along the public domain as required by the ILP.	Yes
	2. The design of footpaths and cycleways located within the road reserve is to be in accordance with Figure 3-11 to Figure 3-21.	Noted, refer to Civil Documentation.	Yes
	3. The minimum width of off-street shared cycle and pedestrian pathways is to be 2.5m.	Shared cycle and pedestrian pathways proposed per civil documentation.	Yes
	4. All pedestrian and cycleway routes and facilities are to be consistent with the Planning Guidelines for Walking and Cycling (DoP & RTA 2004), relevant Council pedestrian and cycling plans	Noted.	-





## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

	<p>and policies, and Council Engineering Design and Construction Specifications.</p> <p>5. Pedestrian and cycle routes and facilities in public spaces are to be safe, well lit, clearly defined, functional and accessible to all.</p> <p>6. Pedestrian and cycle pathways and pedestrian refuge islands are to be designed to be fully accessible by all in terms of access points and gradients, generally in accordance with Australian Standard 1428:1-4.</p> <p>7. Detailed designs for pedestrian and cycle paths are to be submitted with subdivision development applications.</p> <p>8. Pedestrian and cycle pathways that are within road verges or carriageways are to be constructed as part of the road construction works for each subdivision.</p> <p>9. Cycle and pedestrian bridges must be located above the 5% AEP flood level.</p> <p>10. Any through site links that continue the desire line of a road corridor should be the same width of that corridor. All other through site links, pedestrian access paths, or overland flow paths which include a pedestrian connection should be designed in accordance with Figure 3-26.</p>	<p>The pedestrian and cycle ways will be well lit and are designed according to the relevant Standards.</p> <p>Noted.</p> <p>Refer to Civil Documentation.</p> <p>Noted.</p> <p>Not applicable.</p> <p>The through-site link is designed with an adequate width which will allow for ease of pedestrian movement throughout the site.</p>	<p>Yes</p> <p>-</p> <p>Yes</p> <p>-</p> <p>N/A</p> <p>Yes</p>
3.3.7 Temporary vehicular access	<p>1. Where necessary to ensure that access to residential properties is provided in the early stages of development, Council may consent to the construction and operation of temporary access roads.</p> <p>2. Temporary access arrangements must comply with Council's Engineering Specifications and specified provisions of this DCP. Any provisions of this DCP prevail to the extent of any inconsistency.</p> <p>3. Temporary access roads are to remain in operation only until such time as the road network has been developed to provide permanent access to all properties. A section 88B instrument is</p>	<p>The proposal will construct half roads along the future Warrawal Avenue. This will allow access to the site and will not result in any safety or conflict concerns.</p> <p>The half-road widths are designed in accordance with the relevant specifications per the submitted Civil Documentation.</p> <p>Noted.</p>	<p>Yes</p> <p>Yes</p> <p>-</p>



## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

<p>required as part of the subdivision requiring that the temporary access road remains open for as long as necessary to ensure access to all properties.</p> <p>4. The plan of subdivision is to show the location and design of temporary access roads, and the means of transitioning to permanent access arrangements</p> <p>5. Temporary turning circles may be required where roads are to continue onto adjoining properties that are not yet developed.</p> <p>6. A half road is required where a street, as indicated by the precinct Indicative Layout Plan or as otherwise required, is located on the boundary of the property being developed, and where the adjacent lot is not being developed.</p> <p>8. Half width collector roads must be constructed in a manner which provides a carriageway of 5.5m. This can be achieved by providing 0.1m of widening on the adjacent property (with owners consent) or by reducing the landscaped verge on the developed side by 0.1m. The opposite side shall be designed with a wider planting verge.</p> <p>9. A local street may be constructed as one side of Figure 3-15 in instances where the half road will operate in a single direction of travel.</p> <p>11. In all other circumstances a half-width local road may be constructed to provide temporary access to residential development, in accordance with Figure 3-27</p> <p>12. Half-width roads are only permitted where the road is located on the side boundary of the land to be developed.</p> <p>13. The centreline (of the full-width road) is to be located on the boundary.</p> <p>14. The half-width road design is to ensure that runoff from the road pavement is directed away from the adjoining undeveloped property.</p>	<p>Not applicable, no subdivision is proposed.</p>	<p>N/A</p>
	<p>The proposal connects all three roadways.</p>	<p>Yes</p>
	<p>Half road is constructed for the future Warrawal Avenue.</p>	<p>Yes</p>
	<p>The half-width collector road for Warrawal Avenue per the civil plans.</p>	<p>Yes</p>
	<p>The proposal will construct the remaining half of Auger Street (a local road).</p>	<p>Yes</p>
	<p>Not applicable.</p>	<p>N/A</p>
	<p>The half width road is located on the western side boundary of the site and is considered acceptable.</p>	<p>Yes</p>
	<p>Noted, the proposal complies in this regard.</p>	<p>-</p>
	<p>Noted, refer to Civil Documentation.</p>	<p>-</p>

LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE			
	15. The development application plans are to show the vertical alignment of the half-width road relative to existing ground level on the adjoining property, and the applicant is to demonstrate how the halfwidth road will be integrated with adjoining land.	Noted, refer to Civil Documentation which outlines relative level of roadways and surrounding properties.	-
3.4 Construction Environmental Management	1. A Construction Environmental Management Plan (CEMP) is to be submitted to Council or the accredited certifier prior to the issue of a construction certification for subdivision.	Noted, this will be imposed as a condition of consent if required by Council.	Yes
5.0 Centres Development Controls			
5.3.1 Streetscape and architectural design Controls - active frontage and street address	<p>1. Active street fronts, built to the street boundary, are required on the ground level of all retail and commercial development fronting the main street and where applicable, public open space, as identified in the <b>Desired future layout of the Centre</b> figure in the relevant Precinct's Schedule.</p> <p>2. All applications for development in centres are to include a masterplan showing:</p> <ul style="list-style-type: none"> <li>• The location of the proposed development site in the context of the overall centre, and relative to key features of the centre including the main street and other public spaces such as parks, squares and plazas.</li> <li>• How the proposed development fits into the future layout of the centre as shown on the <b>Desired future layout of the Centre</b> figure in the relevant Precinct's Schedule. Where the proposal varies from the desired future layout, the applicant is to demonstrate consistency with the development principles in <b>clause 5.2</b>.</li> <li>• Proposed vehicle and pedestrian access that is consistent with the <b>Traffic circulation and parking</b> figure in the relevant Precinct's Schedule. Where consistency with the <b>Traffic circulation</b> and parking figure is not possible (such as in early stages of the development of the centre prior to construction of key</li> </ul>	<p>The proposal will provide active street frontages to the majority of the Eighth Avenue roadway and to the public piazza. Additional activation is provided along the through-site link and towards the southern façade facing the at-grade carpark. This is considered to be consistent with the Eighth Avenue Neighbourhood Centre.</p> <p>This application is submitted with a site analysis which shows the location of the subject site, in addition to potential future built forms on the neighbouring property.</p> <p>The architectural plans shows the subject site in relationship to the neighbouring properties and ILP Roadways. This is also discussed under Section 5.2.7 of this Statement.</p> <p>The proposed development is consistent with the ILP in terms of roadways and through-site links. A Traffic Report and Civil Documentation is also submitted with this application.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>



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<p>roads) the applicant is to demonstrate consistency with the development principles in clause 5.2.</p> <p>3. Residential, commercial and retail uses on the upper floors are to be designed to overlook streets and other public places to provide passive surveillance.</p> <p>4. The ground and first floor of all buildings on active street frontages are to be built to the front property boundary (ie. a zero front setback) to define the street edge. If the first floor contains residential uses, internal spaces may be set back where balconies are built to the property boundary.</p> <p>5. The primary means of pedestrian access to retail, commercial and upper floor residential uses is to be from the street rather than from the rear or internal areas of the building. Building entries should be prominent, clearly identifiable and accessible.</p> <p>6. Vehicle access to basement level parking or parking located behind buildings must not be from active street frontages.</p> <p>7. All large format retail premises and decked parking areas are to be sleeved with uses that provide an active frontage to the street.</p> <p>8. Blank walls visible from the public domain are to be avoided.</p> <p>9. Retail shops are to have a variety of shop frontage widths and articulation.</p> <p>10. Restaurants, cafes and the like are encouraged to provide openable shop fronts and to make use of footpath areas on active streets.</p> <p>11. On corner sites, active shop fronts are to wrap around the corner and address both street frontages.</p>	<p>The commercial uses on the upper level overlooks the street, public piazza and north-south through-site link.</p>	Yes
	<p>The proposal provides nil to 3.25m front setbacks to all frontages on the ground and first floor and is therefore compliant. Where an increased setback is proposed, this is minor and will allow for covered pedestrian movement.</p>	Yes
	<p>Primary pedestrian access to the site is provided from Eighth Avenue and Auger Street. These will be clearly identifiable from the public domain and maintain clear sightlines.</p>	Yes
	<p>Vehicular access is provided from the rear via the Warrawal Avenue and Auger Street, where the site is not identified as requiring active street frontages. It is noted that these frontages are identified as containing potential vehicle access per the ILP.</p>	Yes
	<p>The supermarket is located internally as accessed from the pedestrian through-site link and will therefore maintain a high level of street activation.</p>	Yes
	<p>Minimal blank facades are proposed. Where these are located facing the Warrawal Avenue, this will be softened through artwork, vertical vegetation and materiality. This is necessary as it will provide access to the loading dock.</p>	Yes
	<p>Refer to architectural set. A variety of commercial and retail floor areas and frontages are provided.</p>	Yes
	<p>Numerous retail and commercial areas are proposed. These can be provided as food and drink premises upon submission of separate applications.</p>	Yes
	<p>The proposal does not provide an active shop front on the sites corner given the topography of the site and location of the public piazza. An</p>	On merit



## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

	<p>12. Developments that have multiple street frontages are to provide entrances to internal/upper floor uses on each street frontage.</p> <p>13. In mixed-use buildings, separate access from the street is required for retail, commercial and residential uses.</p> <p>14. Entrances are to be visible from the street and well lit.</p> <p>15. Security shutters and grilles are not encouraged and any proposed security devices are to be transparent or at least 80% open.</p> <p>16. All buildings on active street frontages are to include awnings above the ground floor for the full length of the street frontage.</p> <p>17. Parking is to be screened by buildings, from the main street and other streets with active frontages, or be below ground.</p>	<p>active shop front on this corner is not considered to be a suitable outcome given is disconnect from the primary pedestrian access points. Access to the internal and upper levels is provided from Eighth Avenue and via Auger Street. No access, excluding a secondary pathway, is provided from Warrawal Avenue given it will primarily service vehicle and loading bay entry.</p> <p>Shared access is provided as only non-residential uses are proposed.</p> <p>Entrances will be well lit and visible from the street frontages. Not proposed.</p> <p>Awnings are provided where practicable and above the active street frontages proposed.</p> <p>The proposal provides at-grade parking which will be visible from the street. Vegetation will be proposed throughout the site and above parking areas to soften the extent of parking area.</p>	<p>On merit</p> <p>Yes</p> <p>Yes</p> <p>N/A</p> <p>Yes</p> <p>Yes</p>
Controls – building facades and awnings	<p>18. Building facades at street level on active frontage streets are to have a minimum of 80% glazing and be open to the street.</p> <p>19. Translucent or obscured glazing is not permitted on active street frontages.</p> <p>20. Signage and advertising material are not to obscure glazing.</p>	<p>The proposal will provide 51% glazed active frontage to Eighth Avenue, including the public piazza. To the future Warrawal Avenue, this does not include any active street frontages due to the topography and loading of services necessary for the development to function. Furthermore, ILP states that a minimum 50% active street frontage is required in which the proposal is considered acceptable. It is also noted that the extent of glazing will adequately activate the streetscapes and ultimately improve the character and safety of the locality.</p> <p>Noted.</p> <p>Signage is proposed as part of this application as it pertain to the supermarket. Signage for other tenancies will form part of separate applications.</p> <p>Condition of consent can be imposed.</p>	<p>On merit</p> <p>-</p> <p>Yes</p> <p>Yes</p>



## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

	<p>21. At night, internal lighting is to fall onto the footpath, or under-awning lighting is to be provided.</p> <p>22. Solid elements are preferably to be finished with rendered masonry, tiles or face brick.</p> <p>23. Coordinated colour schemes are required, and colours and materials are to be consistent with adjoining buildings and the general character of the street.</p> <p>24. Façade articulation is encouraged above the ground floor through the incorporation of balconies, openings and other design elements that modulate the façade, providing rhythm and interest.</p> <p>25. Articulated corners are to be provided to building facades on active street frontages, as identified in the Desired future layout of the Centre figure in the relevant Precinct's Schedule. Articulated elements may include verandahs, awnings, upper level balconies, use of materials or roof designs that accentuate the corner. Articulation elements are to address both street frontages.</p> <p>26. Design of corner buildings on the ground floor is to facilitate free pedestrian movement. Open corners at ground level are encouraged.</p> <p>27. Building height, massing, materials and parapet/roof expression should be used to accentuate corner elements. Council may consider proposals on street corners that do not meet relevant height controls where the design of the building accentuates the corner, creates a landmark and is well designed.</p> <p>28. Any awning over a public footpath will require a Public Road Activity Approval to be issued by the Consent Authority.</p> <p>29. Awnings should be a minimum height of 2.7m (3.2m desirable) above footpath level and generally consistent in form with adjacent awnings.</p> <p>30. The front fascia of the awning is to be set back a minimum of 500mm from the kerb of the street carriageway, including at street corners.</p>	<p>Refer to materials schedule in which the proposal is consistent.</p> <p>The proposal provides a neutral colour scheme and contemporary materials which will support the locality.</p> <p>The proposal provides a highly articulated façade across all elevations as discussed in this Statement. This includes undulation, modulation and materiality.</p> <p>The proposal provides a built form which responds to the corner of Eighth Avenue and Warrawal Avenue. This includes artwork, materiality, texturing, vegetation and proportioning.</p> <p>The built form is splayed at ground level on the corner as to allow ease of movement. Whilst not open, this will not inhibit movement throughout the site.</p> <p>As detailed above, the proposal utilises materiality, texturing, vegetation and proportioning to address the sites corner frontage.</p> <p>Noted.</p> <p>The awning and covered walkway heights change according to the topography of the site and are suitable for users.</p> <p>The proposal complies.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>-</p> <p>Yes</p> <p>Yes</p>
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LIVERPOOL GROWTH CENTRE PRECINCTS DCP– COMPLIANCE TABLE			
	<p>31. Awnings are generally to project horizontally from the building façade and be horizontal along the length of the façade. Stepped awnings are appropriate on sloping streets.</p> <p>32. The design of awnings is to be consistent with adjoining buildings. Awnings that are significantly different in terms of materials, finishes and dimensions will not be permitted.</p> <p>33. Development applications within the centre that propose works in public streets to be undertaken by the developer are to be consistent with any public domain strategy or similar document that applies to the centre.</p> <p>34. All signage and advertising is to be designed in a co-ordinated manner (refer to clause 5.3.4 for detailed controls).</p>	<p>Awnings are horizontal and are integrated into the built form.</p> <p>The proposed awning will allow the neighbouring property to align where it relates to the public piazza.</p> <p>Noted.</p> <p>Signage is proposed for the supermarket. Signage for smaller retail and commercial tenancies will form part of a separate application.</p>	<p>Yes</p> <p>Yes</p> <p>-</p> <p>Yes</p>
Controls – Landscape design and public spaces	<p>35. Parks and plazas are to act as a focal point for the centre and community activities and are to be designed to ensure adaptability and flexibility in use and function over time.</p> <p>36. Plant selection should take into account the following:</p> <ul style="list-style-type: none"> <li>• species which complement remnant native vegetation,</li> <li>• level of on-going maintenance,</li> </ul> <p>scale in relation to the function of the area, solar access and shade, and contribution to the character of the local centre.</p> <p>37. Street tree and open space planting is to provide generous shade for pedestrians in summer and allow for sunlight penetration to street level in winter.</p> <p>38. All paving materials must conform to relevant standards for durability, non-slip textures, strength and surface treatment to withstand use by light automobiles, service vehicles, pedestrians and bicycles.</p>	<p>The proposal provides a public piazza and through-site link which will be focal points of the development and are well designed with quality landscaping.</p> <p>Refer to Landscape Plan.</p> <p>Noted. Refer to Landscape Plan.</p> <p>Refer to Landscape Plan and Access Report.</p>	<p>Yes</p> <p>Yes</p> <p>-</p> <p>Yes</p>





LIVERPOOL GROWTH CENTRE PRECINCTS DCP– COMPLIANCE TABLE			
	<p>39. Paving materials should also be certified colour stable for a period of at least 20 years to ensure a reasonable match to existing paving when damaged sections are replaced.</p> <p>40. All paved areas should be adequately drained and follow 'best practises' in installation, including sub-surface preparation and stormwater management.</p> <p>41. All paved areas must be properly designed to facilitate use by the elderly and disabled.</p>	<p>Noted.</p> <p>Noted.</p> <p>Noted.</p>	<p>-</p> <p>-</p> <p>-</p>
5.3.2 Solar access, weather protection and energy efficiency	<p>1. Parks and plazas are to receive sunlight on a minimum of 50% of their site area between 11am and 2pm on June 21st.</p> <p>2. Building envelopes are to allow for north-south streets to receive 2 hours sunlight between 9am-3pm on June 21st on a minimum of 50% of the eastern or western footpaths; and</p> <p>3. Building envelopes are to allow for east-west streets to receive 2 hours of sunlight between 9am-3pm on June 21st on a minimum of 50% of the southern footpaths.</p> <p>4. Continuous awnings are required to be provided along the ground floor street frontage on active street frontages in accordance with Figure 5-1 and all buildings fronting public open space or squares.</p> <p>5. The design of awnings is to comply with the controls in clause 5.3.1, and:</p> <ul style="list-style-type: none"> <li>• Ensure that the solar access controls in controls 1, 2 and 3 above are achieved.</li> <li>• Ensure that protection from rain and summer sun is provided to a minimum of 75% of footpath areas.</li> </ul> <p>6. The design and orientation of buildings is to consider prevailing south-westerly winds in winter, and active frontages are to be located to maximise shielding from strong winds by buildings.</p>	<p>The proposed public piazza will receive solar access to 100% of its area between 9am and 3pm.</p> <p>The proposal will result in overshadowing to the streetscapes however is considered acceptable given the anticipated increase of density and site orientation.</p> <p>As above, the proposal will maintain adequate solar access to streetscapes as would be reasonably anticipated by the increase of density.</p> <p>Awnings are provided where active street frontages are proposed to Eighth Avenue, the public piazza and north-south through-site link.</p> <p>The proposal is consistent where awnings are proposed.</p> <p>The siting and location of the proposal is predetermined, however has been designed to protect against wind where practicable.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>



## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

<p>7. Uses that are likely to occupy footpaths should be generally located on the southern or western sides of active streets to take advantage of winter sun and protection from winter winds.</p> <p>8. Loading, parking and service areas are preferably to be located on the southern or western sides of buildings, except where the western or southern side of a development site adjoins an active street.</p> <p>9. Residential development within centres is to be generally oriented with living areas and balconies facing north.</p> <p>10. Residential development within centres is to be designed to maximise natural cross-ventilation.</p> <p>11. Large expanses of west-facing glazing, or open shop-fronts facing west, are to be avoided unless the glazing or shop-front is shielded from afternoon sun in summer and cold winter winds by other buildings or awnings.</p> <p>12. Each retail or commercial tenancy is to be separately metered or sub-metered for electricity, gas and water (hot and cold).</p> <p>13. Hot water is to be supplied from solar or heat pump systems. Where these systems can not deliver sufficient hot water to meet demand (e.g. If the roof area is insufficient), gas water heating is preferred.</p> <p>14. Rainwater collected from roof areas is to be used for non-potable uses including toilet flushing, laundries and cleaning.</p> <p>15. All new and refurbished Retail, Commercial and Mixed Use development over the value of \$5 million, shall achieve a minimum Greenstar rating of 4 stars as per the applicable Green Building Council of Australia "as built" rating tool.</p> <p>16. To achieve ESD objectives for new development referred to in control 15:</p> <ul style="list-style-type: none"> <li>• An accredited Greenstar Professional from Green Building Council of Australia (GBCA) is to be engaged on the project.</li> </ul>	<p>Activation of the street frontage is provided as desired by the ILP and is therefore considered acceptable.</p>	<p>Yes</p>
	<p>The parking area and loading bay is provided on the western and southern sides of the site as this is most appropriate for the function of the building.</p>	<p>Yes</p>
	<p>Not applicable.</p>	<p>Yes</p>
	<p>Not applicable.</p>	
	<p>Shopfronts are not oriented to the west to minimise solar impacts during summer.</p>	<p>Yes</p>
	<p>Noted.</p>	<p>-</p>
	<p>Noted.</p>	<p>-</p>
	<p>Refer to Stormwater Plans.</p>	<p>Yes</p>
	<p>Refer to ESD Report submitted with this application.</p>	<p>Yes</p>
	<p>As above, an ESD Report is submitted with this application.</p>	<p>Yes</p>



LIVERPOOL GROWTH CENTRE PRECINCTS DCP– COMPLIANCE TABLE			
	<ul style="list-style-type: none"> <li>• A schedule of achievable Greenstar credits prepared and certified by the accredited Greenstar Professional is to be provided at the lodgement of the Development Application.</li> <li>• Proposed Greenstar measures must be shown on the Development Application documents.</li> <li>• Certificates from suitably qualified structural, hydraulic and mechanical consultants must be provided certifying the ability to incorporate the Green Star commitments at the lodgement of the Development Application.</li> </ul> <p>17. External pedestrian circulation areas are encouraged, rather than internal mall-type buildings. Development that includes internal pedestrian circulation areas should be designed to enable natural ventilation and lighting when weather conditions are appropriate. This may include measures such as openable windows, louvres, skylights and openings on the building perimeter to facilitate natural air circulation. Temporary, moveable or adjustable shade structures are encouraged to provide protection to outdoor or semi-indoor pedestrian circulation areas.</p> <p>18. Retail and commercial tenancies are to be capable of natural ventilation and have access to natural light.</p> <p>19. External glazing or shade structures to commercial and retail development shall be capable of controlling solar ingress into internal spaces. Where necessary, solar ingress control systems shall be dynamically operable via climate control systems for individual tenancies.</p> <p>20. Materials used for construction shall have low Volatile Organic Compounds (VOC) emissions content.</p> <p>21. Timber building materials should be sourced from sustainable suppliers such as products certified by the Forestry Stewardship Council (FSC).</p>	<p>The north-south through-site link is provided as an external circulation path and is partly open to the sky. This includes shading devices as necessary to ensure usability during wet weather conditions and during summer months.</p> <p>All tenancies have frontages to allow the provision of natural light and ventilation.</p> <p>The proposal provides awnings to allow for solar shading during summer and rain protection during winter.</p> <p>Noted.</p> <p>Noted.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>-</p> <p>-</p>



## LIVERPOOL GROWTH CENTRE PRECINCTS DCP– COMPLIANCE TABLE

	<p>22. For construction of developments with a value more than \$10 million, a Construction Environmental Management Plan is to be submitted prior to the issue of a construction certificate, detailing:</p> <ul style="list-style-type: none"> <li>• Measures to reduce the consumption of materials and resources during construction.</li> <li>• The use of recycled or reclaimed materials in construction.</li> <li>• Construction waste minimisation measures, including opportunities to re-use materials on site.</li> <li>• Measures to minimise the use of water and maximise water re-use during construction.</li> <li>• The embodied energy of the main construction materials, options considered to reduce the embodied energy of materials and (if applicable) the reasons for not choosing materials with the least embodied energy.</li> <li>• Training, monitoring and reporting on the compliance of construction contractors with the requirements of the CEMP.</li> </ul>	Noted. Imposed as Condition of Consent.	-
5.3.3 Building bulk, scale and design	<p>1. The maximum allowable depth of residential building envelopes is 22m (max 18m glass line to glass line).</p> <p>2. Floors above the second floor are to be set back a minimum of 4 metres from the boundary of the property with any public street.</p> <p>3. Larger upper floor setbacks from the street may be required to:</p> <ul style="list-style-type: none"> <li>• achieve adequate solar access at street level;</li> <li>• maintain the privacy of dwellings;</li> <li>• maintain view corridors; or</li> <li>• minimise the bulk of the building.</li> </ul> <p>4. Zero side setbacks are required on the ground floor and first floor and the side wall shall contain no windows or other openings (except where the side setback is to a public street, where the façade controls in clause 5.3.1 apply).</p>	<p>Not applicable.</p> <p>The proposed second floor provides a nil setback to the commercial tenancy opposing Eighth Avenue. This control applies to floors above the second floor (being the third floor) and is therefore considered acceptable.</p> <p>The proposed setbacks are considered acceptable as discussed in this Statement.</p> <p>Nil side setbacks are provided to the ground and first floor to the side boundary shared with the neighbouring B1 zone.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>

## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

	5. Zero side setbacks are permitted for the upper floors providing the side wall contains no windows or other openings (except where the side setback is to a public street, where the façade controls in clause 5.3.1 apply). Note: Control 2 above prevails in relation to setbacks to secondary streets in floors above the second floor.	As above, nil side setbacks are proposed to the boundary shared with the neighbouring B1 zone.	Yes
	6. Where windows, balconies or other openings are to be provided on upper floors, the minimum side setback for upper floors is 6 metres from the side property boundary and the minimum separation distance between habitable rooms or balconies is 12 metres.	Not applicable.	N/A
	7. For floors above the fourth floor, the minimum separation distance between buildings is to be 18 metres.	Not applicable.	N/A
	8. Roof forms should not result in excessive bulk or overshadowing.	The proposed roof form will not result in any adverse overshadowing and is contemporary in form.	Yes
	9. All plant and lift over-runs are to be concealed within roof forms to minimise visual impact.	Services and lift overruns situated within the roof and built form and are setback appropriately from the site boundaries.	Yes
	10. The use of roof areas for private / communal open space and gardens is encouraged. Such spaces should be designed to minimise privacy impacts on neighbours.	Not proposed.	N/A
	11. For development in close proximity to a rail corridor, balconies and windows are to be designed so as to prevent objects being thrown onto Railcorp's facilities (refer to the relevant National Construction Code and the Railcorp Electrical Standards).	Not applicable.	N/A
	12. Floor to ceiling heights are to be a minimum of: • Ground floor of all buildings (regardless of use): 3.6m • First floor for retail and/or commercial use: 3.3m • All other retail and/or commercial floors: 3.3m • All other residential floors: 2.4m.	The proposal provides floor to ceiling heights as follows: Lower Ground floor: 3.7m Ground Floor: 4m to 4.8m First Floor: 3.3m for commercial	Yes
5.3.4 Signs	1. Signs are to be designed and located to:	The proposed development is submitted with signage information as it pertains to the Woolworths signs. Separate applications will be submitted	N/A





## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

	<ul style="list-style-type: none"> <li>• Be visually interesting and have a high level of design quality,</li> <li>• Be integrated with the architecture and structure of the building on which they are located;</li> <li>• Be consistent with the scale of the building or the property on which they are located.</li> <li>• Consider existing signs on the building, adjoining buildings or elsewhere in the streetscape, and not obscure views of existing signs or the potential for signs to be viewed on adjoining premises;</li> <li>• Not cover glazed surfaces;</li> <li>• Project minimally from the building.</li> </ul> <p>2. Signs are not to be supported from, hung from or placed on other signs.</p> <p>3. The preferred locations for business or building identification signs are shown on Figure 5-2 and include:</p> <ul style="list-style-type: none"> <li>• Fascia signs, located on the front or side fascia of an awning;</li> <li>• Under-awning signs;</li> <li>• Flush wall mounted signs (e.g. above windows or doors);</li> <li>• Projecting wall signs, where there is no awning or the fixture of the sign to the awning is not appropriate due to the style of the awning.</li> </ul> <p>4. Awning fascia signs are not to project within 500mm of the kerb.</p> <p>5. The minimum clearance from the footpath to the bottom of any sign (apart from flush mounted wall signs) is 2.4 metres.</p> <p>6. Projecting wall signs and under-awning signs are to be perpendicular to the building façade and horizontal.</p> <p>7. Above awning signs (signs that are attached to the top of an awning) are not permitted.</p> <p>8. Flush mounted building identification signs are permitted above the first floor on the building parapet only where they are integrated</p>	<p>for the signage as it pertains to the other retail and commercial tenancies. The following is noted:</p> <p>Signage will be of a high quality design and will be visually interesting as viewed from the public domain.</p> <p>Signage will form an integral part of the overall development and is complementary to the overall design.</p> <p>The signage will not overbear the bulk or scale of the proposed building.</p> <p>Not applicable, as no existing signs are located within the locality.</p> <p>Noted.</p> <p>The signage will not protrude significantly from the building.</p> <p>Not proposed.</p> <p>The proposal includes an appropriate mixture of signs.</p> <p>Noted, the proposal will comply.</p> <p>The proposed signage will not result in a head height of less than 2.4m.</p> <p>Noted, the proposed signs are perpendicular.</p> <p>Not proposed.</p> <p>The proposed building identification signs are flush mounted.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>NA</p> <p>-</p> <p>Yes</p> <p>N/A</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>N/A</p> <p>Yes</p>
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LIVERPOOL GROWTH CENTRE PRECINCTS DCP– COMPLIANCE TABLE			
	<p>with the design of the building and where they do not project more than 100mm from the building. The maximum area of the sign face is 3m².</p> <p>9. The maximum number of signs on each façade of any retail or commercial tenancy is three, and only one sign of each type (fascia, under-awning, projecting wall or flush mounted) is permitted on each façade.</p> <p>10. Under-awning or projecting wall signs are to be a minimum of 3.5 metres apart.</p> <p>11. Signs are not to project beyond the dimensions of the structure to which they are affixed or obscure windows or other openings.</p> <p>12. Free standing signs (signs that are not affixed to a building) are not permitted on active street frontages.</p> <p>13. Flashing, animated or bright neon signage is not permitted.</p> <p>14. Any illuminated signage must comply with AS 4282 – Control of the obtrusive effects of outdoor lighting.</p> <p>15. All buildings are to have clearly displayed and legible street numbering.</p> <p>16. The location of signs is not to obscure views of traffic signs or traffic signals, or have the potential to cause confusion with traffic signs or signals.</p>	<p>The proposed Woolworths is a maximum of three per façade.</p> <p>These are appropriately separated</p> <p>The proposed signage does not protrude beyond the built form.</p> <p>Not proposed.</p> <p>Not proposed.</p> <p>Noted, this can be imposed as a condition of consent.</p> <p>Noted.</p> <p>Signage will not cause conflict or traffic hazards.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>N/A</p> <p>N/A</p> <p>-</p> <p>-</p> <p>Yes</p>
5.3.5 Acoustic and visual privacy	<p>1. Development in the centres must comply with the Office of Environment and Heritage and Council noise attenuation requirements and the controls for visual and acoustic privacy in clause 4.2.9.</p> <p>2. A combination of the following measures is to be used to mitigate the impacts of rail or road traffic noise within centres:</p> <ul style="list-style-type: none"> <li>• setbacks and service roads;</li> <li>• internal dwelling layouts that are designed to minimise noise in living and sleeping areas;</li> <li>• changes in landform;</li> </ul>	<p>Noted. Refer to Section 5.3.8 of this Statement.</p> <p>Refer to Acoustic Report.</p>	<p>Yes</p> <p>Yes</p>





LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE				
		<ul style="list-style-type: none"> <li>• higher than standard fencing constructed with a suitably solid mass; and</li> <li>• locating courtyards and principal private open space areas that will comply with the criteria in clause 4.2.9 away from the noise source.</li> </ul>		
5.3.6	Safety, surveillance and maintenance	<p>1. The principles of Crime Prevention through Environmental Design (CPTED) in Clause 2.5 are applicable to all development within centres.</p> <p>2. Balconies, terraces and other private open spaces are to be oriented to public open spaces to optimise casual surveillance.</p> <p>3. The design of all buildings, fences and landscape elements shall take sight lines, both horizontal and vertical, into consideration to minimize blind spots and promote a sense of security.</p> <p>4. All streets, alleys, bike paths and pedestrian walkways must be adequately lit at all times.</p> <p>5. Lighting is to be installed on all circulation routes and major pedestrian thoroughfares, including under-awning lighting on all awnings.</p> <p>6. Large open areas such as parking lots and public open spaces are to be floodlit.</p> <p>7. Lights should be positioned so that they highlight landmarks and other special building features.</p> <p>8. Lighting fixtures must be sturdy, durable, vandal resistant and easily maintained.</p> <p>9. Fixtures visible from the public domain should be mounted at a height of at least 2.7 metres, and their appearance should complement the architectural and landscape character of the location.</p>	<p>A CPTED Report prepared by <i>Planning Ingenuity</i> is submitted with this application.</p> <p>Not applicable.</p> <p>Noted, no part of the building will obscure sightlines or create areas of concealment.</p> <p>To be provided.</p> <p>Noted.</p> <p>Noted.</p> <p>Noted.</p> <p>Noted.</p> <p>Noted.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>-</p> <p>-</p> <p>-</p> <p>-</p> <p>-</p>





LIVERPOOL GROWTH CENTRE PRECINCTS DCP– COMPLIANCE TABLE			
	<p>10. The installation of lighting should take into account and minimise its impacts on surrounding commercial premises and residential properties.</p> <p>11. Durable and easily cleaned materials should be selected in all areas exposed to the public, and all masonry surfaces to a height of 3 metres should be protected with an approved anti-graffiti treatment.</p> <p>12. Fencing and street plantings should be designed to achieve a balance between screening and security/surveillance.</p> <p>13. Traffic facilities are to be installed to enhance pedestrian safety.</p> <p>14. Safety features such as tactile surfaces and handrails are to be provided in appropriate locations.</p>	<p>Noted.</p> <p>Refer to materials schedule.</p> <p>Refer to Landscape Plan.</p> <p>Refer to Traffic Report.</p> <p>Refer to Access Report.</p>	<p>-</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>
5.3.7 Site servicing	<p>1. Services and structures such as transformers, waste collection, storage and deposit areas, and loading bays are generally to be located to the rear of the property. Where this cannot be achieved services must be integrated into the overall design of buildings and landscaping of the street front through screening measures.</p> <p>2. Service areas are not permitted on active street frontages or adjacent to public parks, plazas or squares.</p> <p>3. Service/delivery vehicles should access service and loading areas using secondary streets (refer to the Traffic Circulation and Parking figure in the relevant Precinct's Schedule for preferred access roads and locations).</p> <p>4. The following controls relate to the screening of services:</p> <ul style="list-style-type: none"> <li>• All services, transformers, storage and deposit areas, and wheeled rubbish bins must be effectively screened from view.</li> <li>• Screening walls or plant masses shall be at least 1.8 metres high, and Council may require higher screens where required to achieve appropriate standards of amenity.</li> </ul>	<p>The proposed services, loading areas, waste storage areas and the like are accessed from Warrawal Avenue. This is integrated into the design and will not be visually jarring from the public domain.</p> <p>The proposal provides access to the waste and loading area from Warrawal Avenue which is not identified as an active frontage. Access is provided from Warrawal Avenue which is not identified as requiring an active frontage and is a secondary street.</p> <p>All services are appropriately integrated into the overall built form.</p> <p>As above.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>



## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

	<ul style="list-style-type: none"> <li>• All screening shall be designed to allow free and easy access to the facilities, as required to permit maintenance and checking by all relevant parties, including service authorities, Council officials, tenants and property owners.</li> <li>• Screening wall materials and plants shall be selected which have no adverse impacts on the operation of the facilities.</li> </ul> <p>5. Service access is permitted from rear lanes, side streets and right of ways for the use of parking, loading docks and waste collection areas.</p> <p>6. Adequate space should be provided for the movement, unloading and loading of service vehicles. All service vehicles should enter and exit any loading area in a forward direction.</p> <p>7. Structures shall be painted according to the required standards of the relevant service authority, in colours that limit their visual impact.</p> <p>8. All air conditioners must be located in areas where any noise and dripping condensation will have minimal impact on the public domain. No roof or wall mounted air conditioners shall be visible from public areas.</p> <p>9. Television antennas and other telecommunication devices are not to be visible from the street.</p>	<p>The services will be easily accessible as required by relevant stakeholders.</p> <p>As mentioned, services are appropriately integrated into the built form.</p> <p>Service access provided from the secondary street.</p> <p>All loading areas are designed to permit vehicular access per the Australian Standards. Refer to the submitted Traffic Report.</p> <p>Noted.</p> <p>All services are screened from the public domain.</p> <p>Not visible from the street.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>-</p> <p>Yes</p> <p>Yes</p>
5.3.8 Traffic circulation, parking and access	<p>1. The pattern of vehicle movement and access to car parking is to be in accordance with the diagram at the Traffic Circulation and Parking figure in the relevant Precinct's Schedule.</p> <p>2. On-site car and bicycle parking is to be provided in accordance with the standards set out in Table 5-1, or standards that apply elsewhere in the Local Government Area, for land uses not listed below. Design of driveways and car parks is to be in accordance with Liverpool DCP 2008 unless this DCP specifies otherwise.</p>	<p>Refer to Traffic Report.</p> <p>The proposed parking is in accordance with the relevant requirements. Refer to Section 5.5.2.</p>	<p>Yes</p> <p>Yes</p>

## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

	<table><tr><th>Land use</th><th>Car parking requirements</th></tr><tr><td>Business premises/office premises</td><td>1 space per 40m² GFA</td></tr><tr><td>Retail premises (less than 200m² GFA)</td><td>1 space per 30m² GFA</td></tr><tr><td>Retail premises (greater than 200m² GFA)</td><td>1 space per 22m² GFA</td></tr><tr><td>Food and drink premises</td><td>1 space per 30m² GFA</td></tr><tr><td>Residential development</td><td>Refer to clause 4.3.5.</td></tr></table>	Land use	Car parking requirements	Business premises/office premises	1 space per 40m² GFA	Retail premises (less than 200m² GFA)	1 space per 30m² GFA	Retail premises (greater than 200m² GFA)	1 space per 22m² GFA	Food and drink premises	1 space per 30m² GFA	Residential development	Refer to clause 4.3.5.		
Land use	Car parking requirements														
Business premises/office premises	1 space per 40m² GFA														
Retail premises (less than 200m² GFA)	1 space per 30m² GFA														
Retail premises (greater than 200m² GFA)	1 space per 22m² GFA														
Food and drink premises	1 space per 30m² GFA														
Residential development	Refer to clause 4.3.5.														
	<p>3. Opportunities for shared parking provision for complementary uses within centres are to be provided. In particular, shared parking provision to cater for rail commuters and retail uses is encouraged. Where retail development is proposed within walking distance (800 metres) of a train station, applicants are to discuss parking arrangements including the potential for shared commuter and retail parking with Railcorp and provide any relevant information as part of the development application.</p> <p>4. In mixed use developments, dedicated on site parking is to be provided for the residential component of the development in accordance with the controls in clause 4.3.5, except where applicants can demonstrate to Council that a lower rate of car parking can meet the demand generated by the residential component. Applicants should consider whether car parking provided for non-residential components of the development could contribute to meeting demand from the residential component, particularly where peak demand generated by the different land uses occurs at different times of the day.</p> <p>5. Secondary streets, rear lanes and right of ways are to be used to provide access to parking areas, loading docks and waste collection areas. Lanes will need to accommodate heavy vehicles where access to loading areas and waste collection is required.</p> <p>6. On-street parking is to be provided on all streets to create a buffer between pedestrian and street traffic and promote casual surveillance.</p> <p>7. Basement, semi-basement or decked parking is preferred over large expanses of at-grade parking.</p>	<p>The proposal does not provide any shared parking spaces.</p> <p>Not applicable, proposal only provides commercial uses.</p> <p>As discussed, access if provided from Warrawal Avenue and Auger Street as anticipated by the ILP.</p> <p>On-street parking is provided along Eighth Avenue and Warrawal Avenue as shown in the Civil Documentation.</p> <p>At-grade parking is provided as this is the most suitable response to the proposed use. This is highly accessible and will encourage use of the centre, whilst reducing on-street parking.</p>	<p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>												





## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

	8. At grade or decked parking areas are to be located behind building lines. Notwithstanding this, Council will consider transitional arrangements for parking where an application is supported by a staging plan that indicates compliance with the above desired parking location principles upon ultimate development.	The proposal provides at-grade parking which is located behind the primary building façade facing Eighth Avenue. This will be softened by landscaping on site and through on-street planting.	Yes
	9. Outdoor parking areas are to be screened and landscaped to minimise their visual dominance within the centre.	As above, landscaping on-site and street tree planting will soften the built form.	Yes
	10. At grade car parks must contain shade tree plantings using tree species and spacing of trees to demonstrate that tree canopies are capable of covering 50% of the car space surface area (excluding car park travel lanes). Submitted plans are to illustrate the estimated extent of tree canopies at maturity.	The at-grade parking will provide for vegetation which will equate to canopy cover of 15.7% of the car space surface area, excluding car park travel lanes.	Yes
	11. Bicycle parking is to be in secure and accessible locations. Bicycle parking for employees is to have weather protection.	Bicycle parking is provided within the public piazza and at grade parking area.	Yes
	12. The parking area per vehicle is to be in accordance with AS 2890:1. Provision for service vehicles is to be in accordance with AS2890.2.	Refer to Traffic Report which demonstrates compliance with the Australian Standards.	Yes
	13. The main street and streets that have active frontages are to be designed generally in accordance with Figure 5-3, and are to have the following minimum dimensions: • Footpaths (from back of kerb to the boundary of the road reserve) are to be a minimum of 4.5 metres wide. Additional width may be necessary at public transport facilities such as bus stops. • Carriageways are to be a minimum of 6.5m wide with sufficient capacity for kerbside parking/cycle lanes and at least one traffic lane with a minimum width of 3.5 metres.	The proposal activates the frontages as required by the ILP. The dimensions are as follows: Refer to civil plans.	Yes
	14. The design of the main street and other town centre streets is to effectively transition from the design required within the town centre to the design required in the surrounding urban areas (refer to Figure 3-11 to Figure 3-18 for typical street designs in residential areas)	Refer to civil plans.	Yes
		The proposal is in accordance with the setback requirements of the relevant neighbourhood and will provide an appropriate transition.	Yes



LIVERPOOL GROWTH CENTRE PRECINCTS DCP– COMPLIANCE TABLE			
	15. Where the kerb side lane is a dedicated parking lane (ie. not used as a traffic lane during peak periods), the kerb and footpath is to extend into the parking lane at signalised intersections and key pedestrian crossing locations.	Noted.	-
	16. Specific road cross-sections for certain streets may be contained in the relevant Precinct Schedule, and prevail over the controls above where there is any inconsistency.	Noted.	-
Schedule 1 – Austral & Leppington North Precincts			
3.1 Centres within the Austral and Leppington Precinct	<p><u>3.1.1 Hierarchy and function of local and neighbourhood centres</u></p> <p>1. The location of centres is to be as shown on <b>Figure 3-1</b>.</p> <p>2. The Austral Local Centre is to contain a mix of retail and local commercial land uses, to a maximum gross leaseable floorspace of 30,000m<sup>2</sup>.</p> <p>3. The Gurner Avenue Neighbourhood Centre is to contain a mix of retail and local commercial land uses, to service a population in the order of 10,000 people, and to a maximum gross leaseable floorspace of 10,000m<sup>2</sup>.</p> <p>4. The Eighth Avenue Neighbourhood Centre is to contain a mix of retail and local commercial land uses, to service a population in the order of 10,000 people, and to a maximum gross leaseable floorspace of 10,000m<sup>2</sup>.</p> <p>5. The Austral Neighbourhood Centre is to expand on the existing retail and community uses to cater for a population in the order of 10,000 people and to a maximum gross leaseable floorspace of 10,000m<sup>2</sup>.</p> <p>6. All local and Neighbourhood Centres are to contain a mix of large floorplate and specialty retail uses.</p> <p>7. Leppington Major Centre is to be the focus of higher order retail, commercial, entertainment, civic and cultural activities in the South West Growth Centre, and will be supported by the Local and Neighbourhood Centres.</p>	<p>Noted. The site is located in the Eighth Avenue Neighbourhood Centre. Not applicable.</p> <p>Not applicable.</p> <p>The proposed development does not exceed the maximum 10,000m<sup>2</sup> commercial floor area requirement and is therefore acceptable. This control is not to be enforced on a pro-rata basis.</p> <p>Not applicable.</p> <p>The proposal provides a mixture of commercial floor spaces which will encourage a range of uses. Not applicable.</p>	<p>Yes N/A</p> <p>Yes</p> <p>Yes</p> <p>N/A</p> <p>Yes N/A</p>



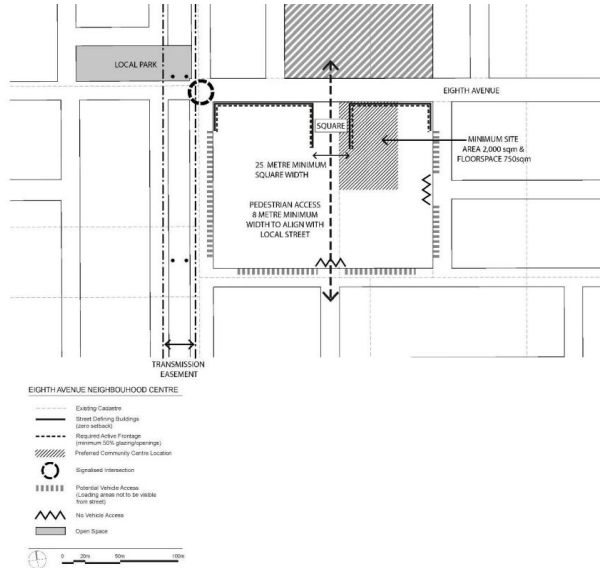


LIVERPOOL GROWTH CENTRE PRECINCTS DCP– COMPLIANCE TABLE			
	8. Neighbourhood shops, located outside the defined centres, are encouraged where they serve a particular market need and can be integrated with surrounding land uses. Out of centre retailing is not encouraged where it is inconsistent with the Indicative Layout Plan or where it would jeopardise the function and viability of the Local and Neighbourhood Centres or the Leppington Major Centre.	Not applicable, site is located within a Neighbourhood Centre. The proposal will include a variety of uses and 'shops' are permitted with consent.	Yes
3.1.4 Eighth Avenue Neighbourhood Centre	<p>1. The main street in the Eighth Avenue Neighbourhood Centre is Eighth Avenue.</p> <p>2. Active Street Frontages are to be provided to the main street consistent with Figure 3-5.</p> <p>3. On street parking is to be provided on Eighth Avenue except where turning lanes or bus bays prevent parking.</p> <p>4. Vehicular access to car parks and loading bays is to be primarily via perimeter streets to the west and south of the centre.</p> <p>5. Loading bays are to be located behind buildings fronting Eighth Avenue.</p> <p>6. Loading areas that are adjacent to residential zoned land are to include visual and acoustic screening to protect the amenity of residents.</p> <p>7. Loading areas are to be located to not be visible from parks and sporting fields.</p>	<p>The proposal will address Eighth Avenue.</p> <p>As above, the proposal will address Eighth Avenue and activate the street frontage. The corner of the site is not activated, however, is considered acceptable as discussed in this Statement.</p> <p>Refer to Civil Documentation.</p> <p>Vehicular access to the on-site loading bays and parking areas is provided from Warrawal Avenue and Auger Street.</p> <p>Loading bays are integrated into the built form and are located behind Eighth Avenue.</p> <p>Loading areas are integrated into the built form and will have no impact on the surrounding residential zones. An Acoustic Report is submitted with this application.</p> <p>Loading areas is generally obscured from the local park to the north-west of the site.</p>	<p>Yes</p> <p>On merit</p> <p>Yes</p> <p>Yes</p> <p>Yes</p> <p>Yes</p>





## LIVERPOOL GROWTH CENTRE PRECINCTS DCP- COMPLIANCE TABLE

		<p>The proposal is generally in accordance with the ILP Layout. The proposal will provide a public piazza and north-south through-site link. This is discussed in Section 5.2.7 of this Statement.</p>	<p>Yes</p>
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